

CITY OF GOSHEN STANDARD ROAD DETAILS

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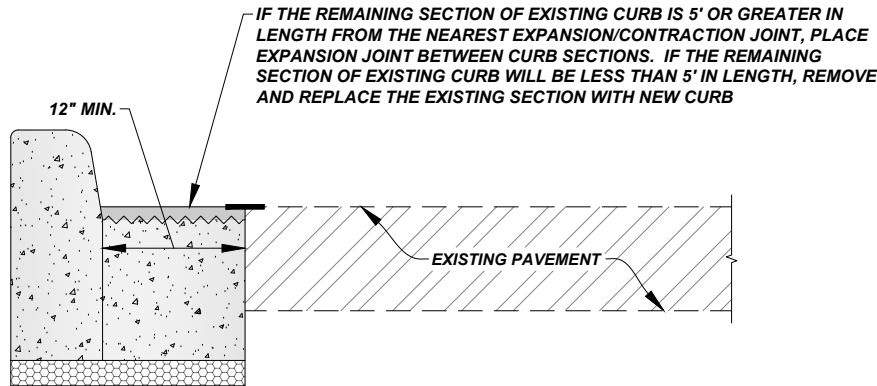
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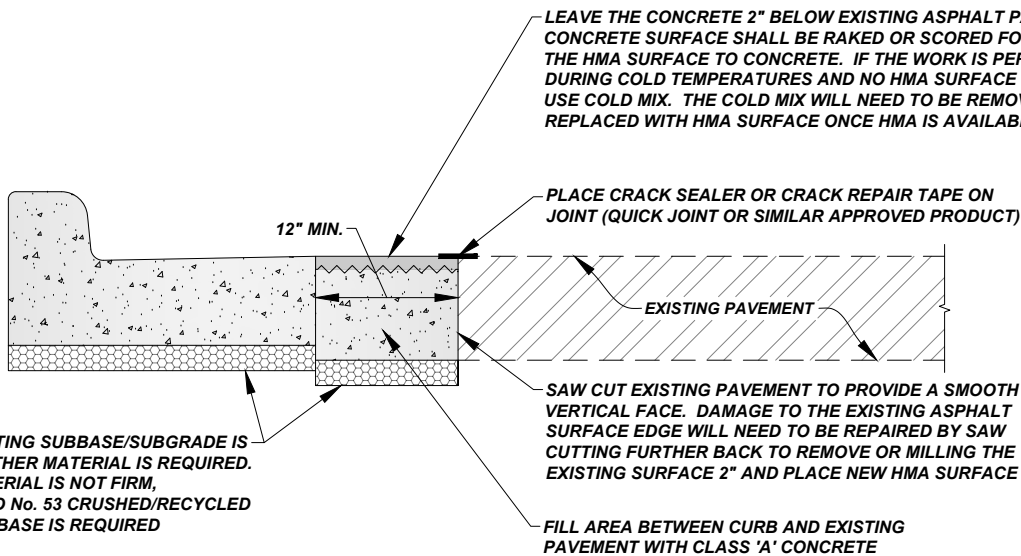
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IF THE REMAINING SECTION OF EXISTING CURB IS 5' OR GREATER IN LENGTH FROM THE NEAREST EXPANSION/CONTRACTION JOINT, PLACE EXPANSION JOINT BETWEEN CURB SECTIONS. IF THE REMAINING SECTION OF EXISTING CURB WILL BE LESS THAN 5' IN LENGTH, REMOVE AND REPLACE THE EXISTING SECTION WITH NEW CURB



IF THE EXISTING SUBBASE/SUBGRADE IS FIRM, NO OTHER MATERIAL IS REQUIRED. IF THE MATERIAL IS NOT FIRM, COMPACTED No. 53 CRUSHED/RECYCLED CONCRETE BASE IS REQUIRED

LEAVE THE CONCRETE 2\"/>

PLACE CRACK SEALER OR CRACK REPAIR TAPE ON JOINT (QUICK JOINT OR SIMILAR APPROVED PRODUCT)

SAW CUT EXISTING PAVEMENT TO PROVIDE A SMOOTH VERTICAL FACE. DAMAGE TO THE EXISTING ASPHALT SURFACE EDGE WILL NEED TO BE REPAIRED BY SAW CUTTING FURTHER BACK TO REMOVE OR MILLING THE EXISTING SURFACE 2\"/>

FILL AREA BETWEEN CURB AND EXISTING PAVEMENT WITH CLASS 'A' CONCRETE

NOTES:

1. CLASS 'A' CONCRETE, 4,000 PSI
2. CURB EXPANSION JOINT LOCATIONS:
STRAIGHT CURB SECTION - EVERY 40'
ROADWAY CURVE - BEGINNING POINT, MID POINT, END POINT AND EVERY 40' ALONG CURVE
INTERSECTION RADIUS - BEGINNING POINT, MID POINT, END POINT WITHOUT ADA RAMP OR SIDEWALK.
EACH SIDE OF RAMP
CURB CASTING - 4' FROM EACH SIDE OF THE CASTING
EXPANSION MATERIAL MUST BE FULL DEPTH AND WIDTH OF CURB SECTION OR WILL REQUIRE REPLACED
3. CONTRACTION JOINT LOCATIONS:
REQUIRED EVERY 10' AT 1/3 THE DEPTH, D/3, ALONG STRAIGHT CURB SECTIONS, ROADWAY CURVES
AND INTERSECTION RADII
SAW CUT CONTRACTION JOINTS WITHIN 24 HOURS OF PLACEMENT
NO TOOLING OF CONTRACTION JOINTS IS ALLOWED
4. REMOVE ALL EXCESS MATERIAL ALONG THE FACE OF THE CURB PRIOR TO MATERIAL SETTING TO ALLOW A CLEAN VERTICAL FACE TO PAVE OR POUR AGAINST. REMOVE EXCESS MATERIAL ALONG BACKSIDE OF CURB, AT DRIVE APPROACHES AND SIDEWALKS, TO ALLOW A CLEAN VERTICAL FACE FOR EXPANSION MATERIAL BETWEEN CONCRETE POURS
5. THE SUB-BASE/SUB-GRADE CAN NOT BE FROZEN OR WET WHEN THE CONCRETE IS PLACED. IF THE TEMPERATURES WILL CAUSE FROST IN THE GROUND, BLANKETS NEED TO BE PLACED OVER THE SUB-BASE/SUB-GRADE TO PREVENT FREEZING PRIOR TO PLACING THE CONCRETE
6. BROOM FINISH THE CONCRETE
APPLY WHITE PIGMENT SEALER
APPLY A DOUBLE COAT OF SEALER FROM SEPT. 1 THROUGH MARCH 31 TO PROTECT AGAINST WINTER SALT
7. RESTORE YARD AREAS WITH 4\"/>

NOTE:

PREFERRED METHOD - REMOVE EXISTING CURB WITHOUT DAMAGING THE ASPHALT PAVEMENT THEN POUR THE NEW CURB AGAINST THE EXISTING ASPHALT PAVEMENT

ALTERNATE METHOD - IF EXISTING PAVEMENT IS DAMAGED, REPLACEMENT SHALL BE AS DETAILED

CONCRETE CURB REPAIR/REPLACEMENT

NOT TO SCALE

The City Of Goshen
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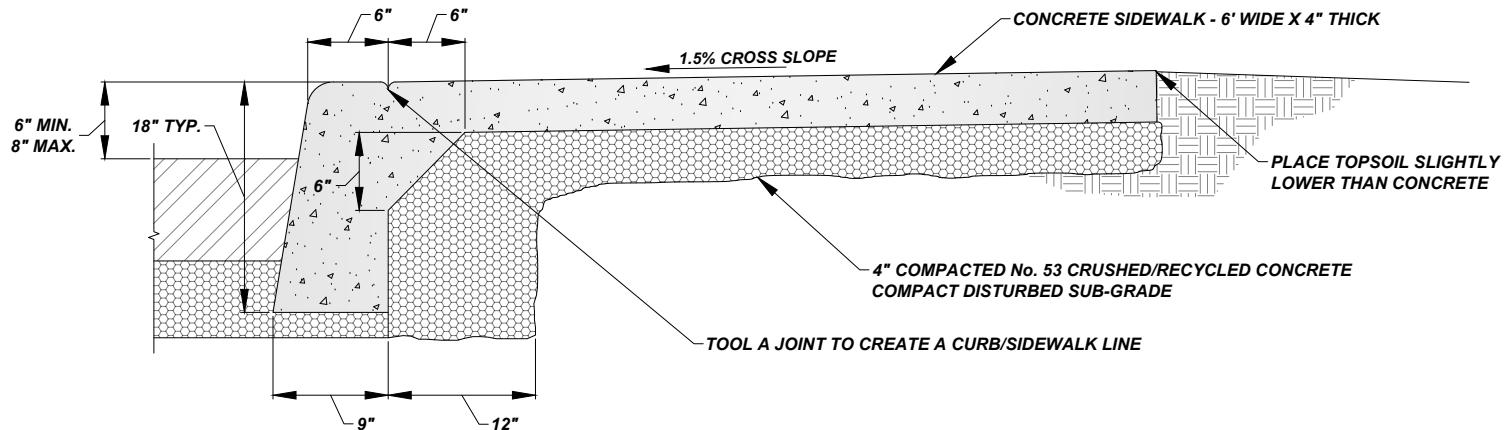
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03/27/2025					

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman	Approved By: D. Sailor, P.E.
Drawing Number: RD-101	

1. CLASS 'A' CONCRETE, 4,000 PSI
2. EXPANSION JOINT LOCATIONS:
STRAIGHT CURB AND SECTION - EVERY 40'
ROADWAY CURVE - BEGINNING POINT, MID POINT, END POINT AND EVERY 40' ALONG CURVE
INTERSECTION RADIUS - BEGINNING POINT, MID POINT, END POINT WITHOUT ADA RAMP OR SIDEWALK. EACH SIDE OF RAMP
CURB CASTING - 4' FROM EACH SIDE OF THE CASTING
SIDEWALK - EVERY 42'
EXPANSION MATERIAL MUST BE FULL DEPTH AND WIDTH OF CURB SECTION OR WILL REQUIRE REPLACED
3. CONTRACTION JOINT LOCATIONS:
REQUIRED EVERY 10' (CURB) AND 6' (SIDEWALK) AT 1/3 THE DEPTH, D/3, ALONG STRAIGHT CURB SECTIONS, ROADWAY CURVES AND INTERSECTION RADII. SAW CUT CONTRACTION JOINTS WITHIN 24 HOURS OF PLACEMENT. NO TOOLING OF CONTRACTION JOINTS IS ALLOWED
4. REMOVE ALL EXCESS MATERIAL ALONG THE FACE OF THE CURB PRIOR TO MATERIAL SETTING TO ALLOW A CLEAN VERTICAL FACE TO PAVE OR POUR AGAINST. REMOVE EXCESS MATERIAL ALONG BACKSIDE OF CURB, AT DRIVE APPROACHES AND SIDEWALKS, TO ALLOW A CLEAN VERTICAL FACE FOR EXPANSION MATERIAL BETWEEN CONCRETE POURS
5. THE SUB-BASE/SUB-GRADE CAN NOT BE FROZEN OR WET WHEN THE CONCRETE IS PLACED. IF THE TEMPERATURES WILL CAUSE FROST IN THE GROUND, BLANKETS NEED TO BE PLACED OVER THE SUB-BASE/SUB-GRADE TO PREVENT FREEZING PRIOR TO PLACING THE CONCRETE
6. BROOM FINISH THE CONCRETE
APPLY WHITE PIGMENT SEALER
APPLY A DOUBLE COAT OF SEALER FROM SEPT. 1 THROUGH MARCH 31 TO PROTECT AGAINST WINTER SALT
7. RESTORE YARD AREAS WITH 4" OF CLEAN TOPSOIL, INDOT TYPE "U" WITHOUT CLOVER, COVER WITH EROSION CONTROL BLANKET. THE CONTRACTOR IS RESPONSIBLE TO ACHIEVE 70% EVEN COVERAGE

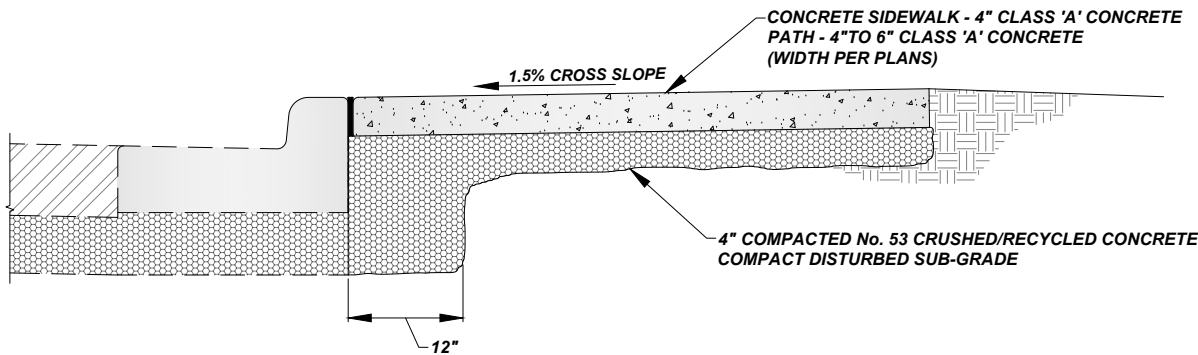
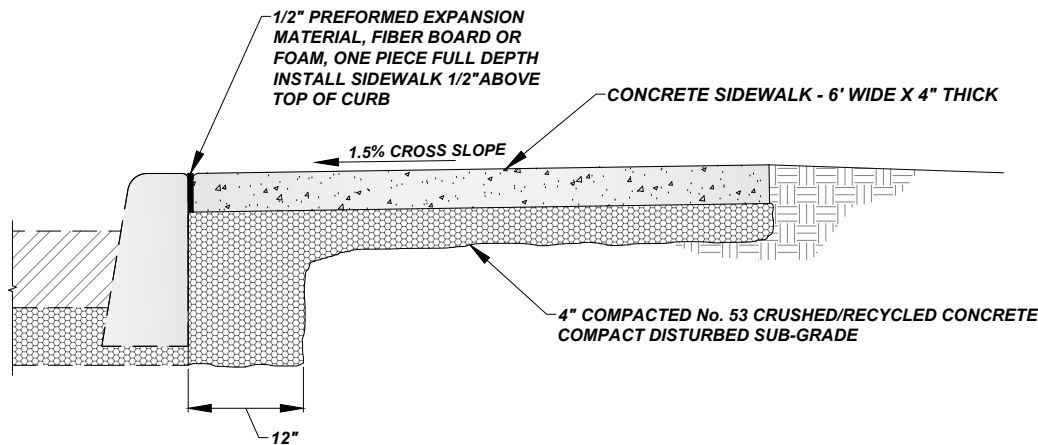


NOT TO SCALE

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SIDEWALK ADJACENT TO CONCRETE CURB

NOT TO SCALE

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6. BROOM FINISH THE CONCRETE
APPLY WHITE PIGMENT SEALER
APPLY A DOUBLE COAT OF SEALER FROM SEPT. 1 THROUGH MARCH 31 TO PROTECT AGAINST WINTER SALT
7. RESTORE YARD AREAS WITH 4" OF CLEAN TOPSOIL, INDOT TYPE 'U' WITHOUT CLOVER, COVER WITH EROSION CONTROL BLANKET. THE CONTRACTOR IS RESPONSIBLE TO ACHIEVE 70% EVEN COVERAGE

The City Of Goshen
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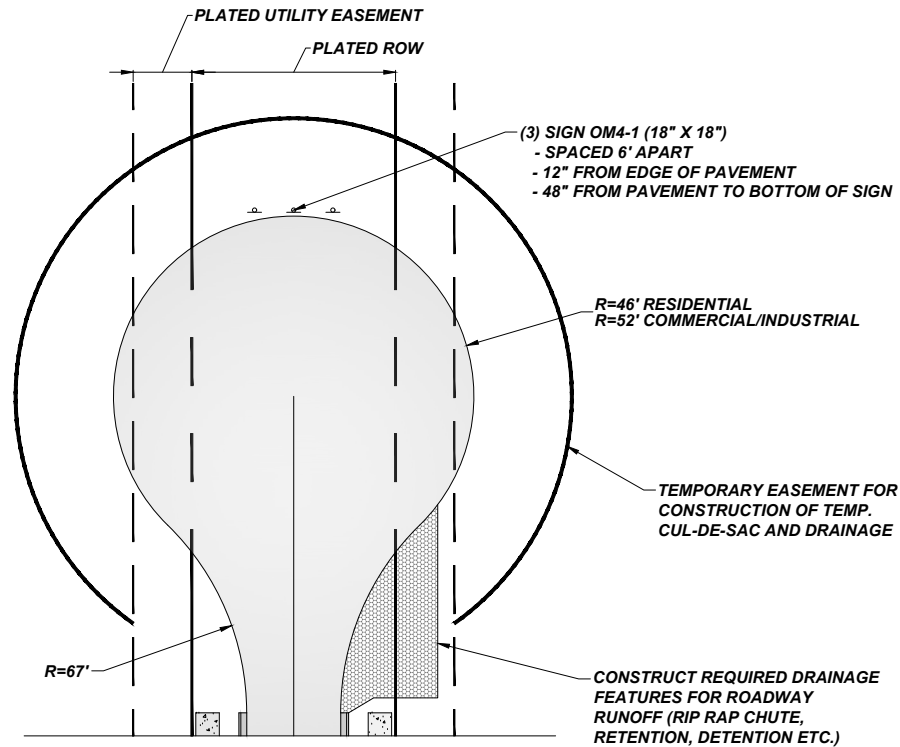
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CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.

Drawing Number:
RD-103



NOTES:

1. **TEMPORARY CUL-DE-SAC SECTION:**
1.5" HMA SURFACE OVER
3.0 HMA BASE OVER
4.0" No. 53 RECYCLED/CRUSHED CONCRETE
2. ALL ROADWAY RUNOFF REACHING THE TEMPORARY CUL-DE-SAC MUST BE ACCOUNTED FOR WITH PROPER DRAINAGE FEATURES CONSTRUCTED TO CONTAIN THE RUNOFF. NO IMPACT TO SURROUNDING PROPERTIES WILL BE ALLOWED
3. PLACE SIGN OM4-1 AT THE END OF THE TEMPORARY CUL-DE-SAC AS SHOWN IN THE DETAIL
4. THE DEVELOPER WILL BE REQUIRED TO MAINTAIN ALL ITEMS WITHIN THE TEMPORARY CUL-DE-SAC, INCLUDING PAVEMENT, DRAINAGE, VEGETATION, SIGNS AND SNOW REMOVAL

TEMPORARY RESIDENTIAL CUL-DE-SAC

NOT TO SCALE

The City Of Goshen
Department of Public Works & Safety
Office of Engineering

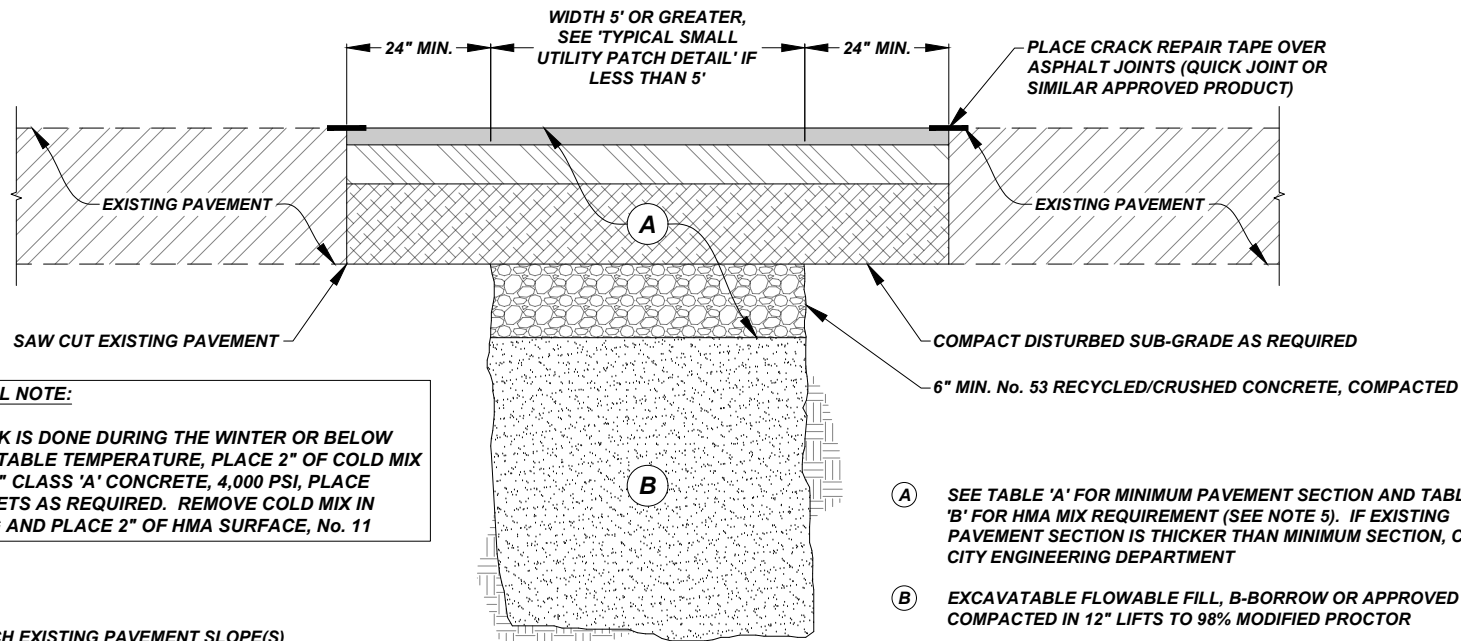
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CITY OF GOSHEN, INDIANA
STANDARD DETAIL

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Approved By: D. Sailor, P.E.
Drawing Number: **RD-104**



SPECIAL NOTE:

IF WORK IS DONE DURING THE WINTER OR BELOW ACCEPTABLE TEMPERATURE, PLACE 2" OF COLD MIX OVER 6" CLASS 'A' CONCRETE, 4,000 PSI, PLACE BLANKETS AS REQUIRED. REMOVE COLD MIX IN SPRING AND PLACE 2" OF HMA SURFACE, No. 11

NOTES:

1. MATCH EXISTING PAVEMENT SLOPE(S)
2. SAW CUT EXISTING PAVEMENT PRIOR TO REMOVAL TO PROVIDE A CLEAN VERTICAL FACE. SAW CUT EDGE(S) AGAIN IF DAMAGED PRIOR TO PLACING PATCH. IF THE HMA SURFACE HAS BEEN DAMAGED BEYOND THE SAW CUT(S) AND/OR MATERIAL HAS BEEN LOST UNDER THE EXISTING PAVEMENT, INCREASE THE PATCH DIMENSIONS TO INCLUDE THESE AREAS
3. TRENCH SPOILS TO BE REMOVED FROM SITE IF UNSUITABLE FOR BACKFILL. A CITY REPRESENTATIVE SHALL DETERMINE IF SPOILS ARE SUITABLE FOR BACKFILL
4. PATCH MAY BE MODIFIED TO CONTAIN ONLY HMA BASE AND HMA SURFACE. REMOVAL OF THE HMA BINDER LAYER MAY BE ELIMINATED WITH APPROVAL FROM THE CITY ENGINEERING DEPARTMENT. HMA BASE LIFTS MAY NOT EXCEED 4"
5. COMPACT THE HMA BASE, HMA BINDER AND HMA SURFACE WITH A DOUBLE DRUM ROLLER UNLESS AN ALTERNATIVE METHOD HAS BEEN APPROVED BY THE ENGINEERING DEPARTMENT
6. RESTORE DAMAGED OR REMOVED PAVEMENT MARKINGS WITH LIKE MATERIAL. A CITY REPRESENTATIVE SHALL APPROVE MATERIAL AND METHOD OF INSTALLATION
7. STREET RESTRICTIONS OR CLOSURES FOR COLLECTOR OR ARTERIAL ROADWAY MUST BE APPROVED BY THE CITY BOARD OF WORKS. CONTACT THE CITY ENGINEERING DEPARTMENT AT (574) 534-2201 FOR THE SUBMITTAL REQUIREMENTS AND DATES FOR THE BOARD OF WORKS MEETING
8. WORK WITHIN THE CITY RIGHT OF WAY REQUIRES A PERMIT ISSUED BY THE CITY ENGINEERING DEPARTMENT. ACCESS THE CITY'S PERMIT PORTAL AT GOSHENINDIANA.ORG
9. A CITY REPRESENTATIVE MUST INSPECT THE BACKFILL PROCESS AND PLACEMENT OF THE PATCH CALL THE CITY ENGINEERING DEPARTMENT TO SCHEDULE THE INSPECTIONS AT (574) 534-2201, 48 HOUR NOTICE REQUIRED
10. IF A CITY UTILITY WAS REPAIRED AS PART OF THE WORK, A CITY REPRESENTATIVE MUST INSPECT THE REPAIR PRIOR TO BACKFILLING. IF AN INSPECTION WAS NOT PERFORMED, THE CONTRACTOR WILL BE REQUIRED TO EXPOSE THE REPAIR FOR INSPECTION

LARGE ASPHALT PAVEMENT PATCH
NOT TO SCALE

The City Of Goshen
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CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.

Drawing Number:
RD-201

TABLE 'A' - MINIMUM ACCEPTABLE PAVEMENT SECTION						
STREET CLASS		COMPACTED SUB-BASE (No. 53)	HOT MIX ASPHALT (HMA)			CONCRETE (PCC)
			BASE (No. 5)	BINDER (No. 8)	SURFAC E (No. 11)	
FLEXIBLE	MINOR ARTERIAL	6"	6"	2.5"	1.5"	-
	COLLECTOR & INDUSTRIAL	6"	4"	2.5"	1.5"	-
	LOCAL	6"	3"	2.5"	1.5"	-
RIGID	MINOR ARTERIAL	6"	-	-	-	9"
	COLLECTOR & INDUSTRIAL	6"	-	-	-	9"
	LOCAL	6"	-	-	-	6"

TABLE 'B' - AGGREGATE GRADATION FOR HMA MIXES			
SIEVE SIZE	BASE MIXTURES No. 5	BINDER MIXTURES No. 8	SURFACE MIXTURES No. 11
1.5 in (37.5 mm)	100		
1 in (25.0 mm)	90 -99	100	
3/4 in (19.0 mm)	<90	90 -99	
1/2 in (12.5 mm)		<90	100
3/8 in (9.5 mm)			90 - 99
No. 4			<90
No. 8	23 (±6)	29 (±6)	57 (±6)
No. 16			
No. 30			
No. 50			
No. 100			
No. 200	1 - 7	2 - 8	2 - 6
PERCENT OF BITUMINOUS	4.5 (±0.5)	4.8 (±0.5)	6.1 (±0.5)
PERCENT OF MOISTURE, MAX	0.3	0.3	0.3

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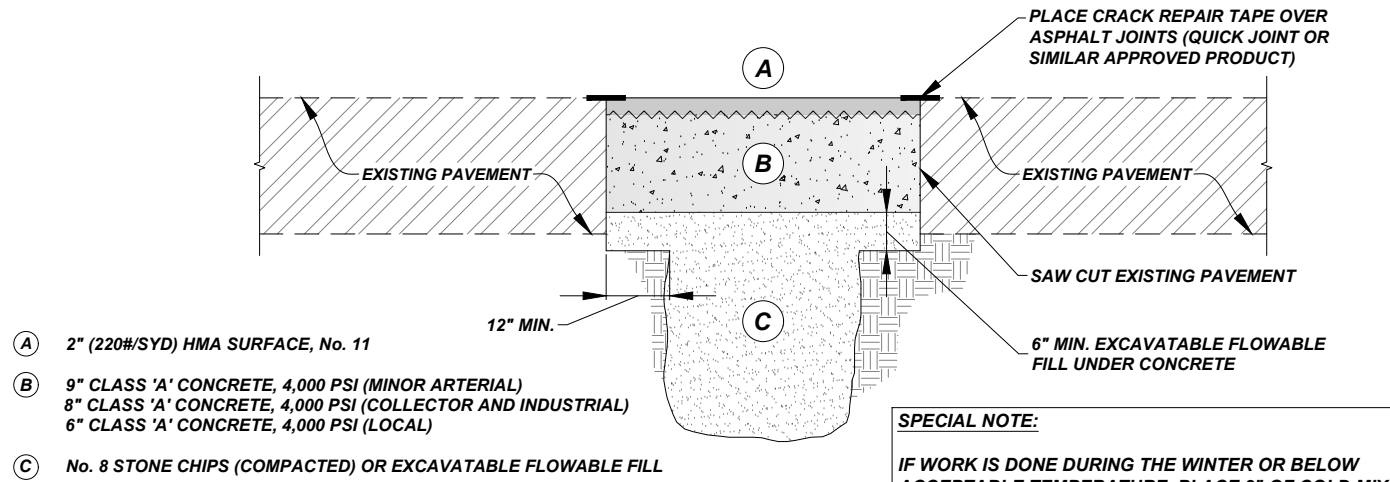
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CITY OF GOSHEN, INDIANA
STANDARD DETAIL

PAGE 2 OF 2

Drafted By: J. Hoffman	Approved By: D. Sailor, P.E.
Drawing Number: RD-201	



NOTES:

1. MATCH EXISTING PAVEMENT SLOPE(S)
2. SAW CUT EXISTING PAVEMENT PRIOR TO REMOVAL TO PROVIDE A CLEAN VERTICAL FACE. SAW CUT EDGE(S) AGAIN IF DAMAGED PRIOR TO PLACING PATCH. IF THE HMA SURFACE HAS BEEN DAMAGED BEYOND THE SAW CUT(S) AND/OR MATERIAL HAS BEEN LOST UNDER THE EXISTING PAVEMENT, INCREASE THE PATCH DIMENSIONS TO INCLUDE THESE AREAS
3. TRENCH SPOILS TO BE REMOVED FROM SITE IF UNSUITABLE FOR BACKFILL. A CITY REPRESENTATIVE SHALL DETERMINE IF SPOILS ARE SUITABLE FOR BACKFILL
4. COMPACT HMA SURFACE WITH A DOUBLE DRUM ROLLER UNLESS AN ALTERNATIVE METHOD HAS BEEN APPROVED BY THE ENGINEERING DEPARTMENT
5. RESTORE DAMAGED OR REMOVED PAVEMENT MARKINGS WITH LIKE MATERIAL. A CITY REPRESENTATIVE SHALL APPROVE MATERIAL AND METHOD OF INSTALLATION
6. STREET RESTRICTIONS OR CLOSURES FOR COLLECTOR OR ARTERIAL ROADWAY MUST BE APPROVED BY THE CITY BOARD OF WORKS. CONTACT THE CITY ENGINEERING DEPARTMENT AT (574) 534-2201 FOR THE SUBMITTAL REQUIREMENTS AND DATES FOR THE BOARD OF WORKS MEETING
7. WORK WITHIN THE CITY RIGHT OF WAY REQUIRES A PERMIT ISSUED BY THE CITY ENGINEERING DEPARTMENT. ACCESS THE CITY'S PERMIT PORTAL AT GOSHENINDIANA.ORG
8. A CITY REPRESENTATIVE MUST INSPECT THE BACKFILL PROCESS AND PLACEMENT OF THE PATCH
CALL THE CITY ENGINEERING DEPARTMENT TO SCHEDULE THE INSPECTIONS AT (574) 534-2201, 48 HOUR NOTICE REQUIRED
9. IF A CITY UTILITY WAS REPAIRED AS PART OF THE WORK, A CITY REPRESENTATIVE MUST INSPECT THE REPAIR PRIOR TO BACKFILLING. IF AN INSPECTION WAS NOT PERFORMED, THE CONTRACTOR WILL BE REQUIRED TO EXPOSE THE REPAIR FOR INSPECTION

SPECIAL NOTE:

IF WORK IS DONE DURING THE WINTER OR BELOW ACCEPTABLE TEMPERATURE, PLACE 2" OF COLD MIX OVER CONCRETE, PLACE BLANKETS AS REQUIRED. REMOVE COLD MIX IN SPRING AND PLACE 2" OF HMA SURFACE, No. 11

SMALL ASPHALT PAVEMENT PATCH

NOT TO SCALE

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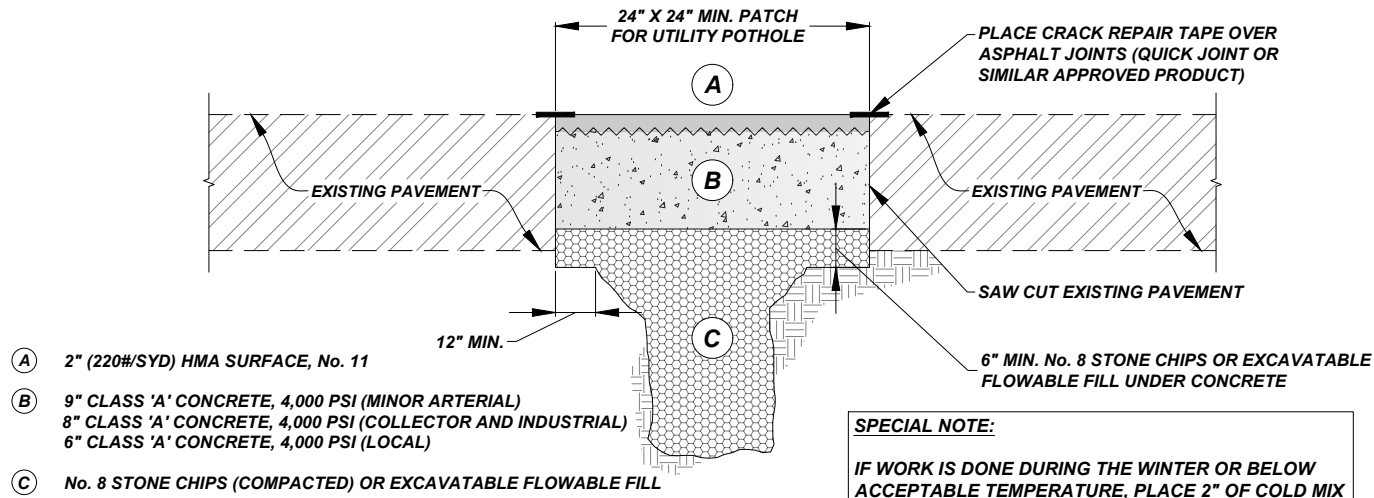
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CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.

Drawing Number:
RD-202



SPECIAL NOTE:

IF WORK IS DONE DURING THE WINTER OR BELOW ACCEPTABLE TEMPERATURE, PLACE 2" OF COLD MIX OVER CONCRETE, PLACE BLANKETS AS REQUIRED. REMOVE COLD MIX IN SPRING AND PLACE 2" OF HMA SURFACE, No. 11

NOTES:

1. MATCH EXISTING PAVEMENT SLOPE(S)
2. SAW CUT EXISTING PAVEMENT PRIOR TO REMOVAL TO PROVIDE A CLEAN VERTICAL FACE. SAW CUT EDGE(S) AGAIN IF DAMAGED PRIOR TO PLACING PATCH. IF THE HMA SURFACE HAS BEEN DAMAGED BEYOND THE SAW CUT(S) AND/OR MATERIAL HAS BEEN LOST UNDER THE EXISTING PAVEMENT, INCREASE THE PATCH DIMENSIONS TO INCLUDE THESE AREAS
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4. PATCH MAY BE MODIFIED TO CONTAIN ONLY HMA BASE AND HMA SURFACE. REMOVAL OF THE HMA BINDER LAYER MAY BE ELIMINATED WITH APPROVAL FROM THE CITY ENGINEERING DEPARTMENT. HMA BASE LIFTS MAY NOT EXCEED 4"
5. HMA SURFACE WITH A DOUBLE DRUM ROLLER UNLESS AN ALTERNATIVE METHOD HAS BEEN APPROVED BY THE ENGINEERING DEPARTMENT
6. RESTORE DAMAGED OR REMOVED PAVEMENT MARKINGS WITH LIKE MATERIAL. A CITY REPRESENTATIVE SHALL APPROVE MATERIAL AND METHOD OF INSTALLATION
7. STREET RESTRICTIONS OR CLOSURES FOR COLLECTOR OR ARTERIAL ROADWAY MUST BE APPROVED BY THE CITY BOARD OF WORKS. CONTACT THE CITY ENGINEERING DEPARTMENT AT (574) 534-2201 FOR THE SUBMITTAL REQUIREMENTS AND DATES FOR THE BOARD OF WORKS MEETING
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UTILITY POTHOLE ASPHALT PAVEMENT PATCH

NOT TO SCALE

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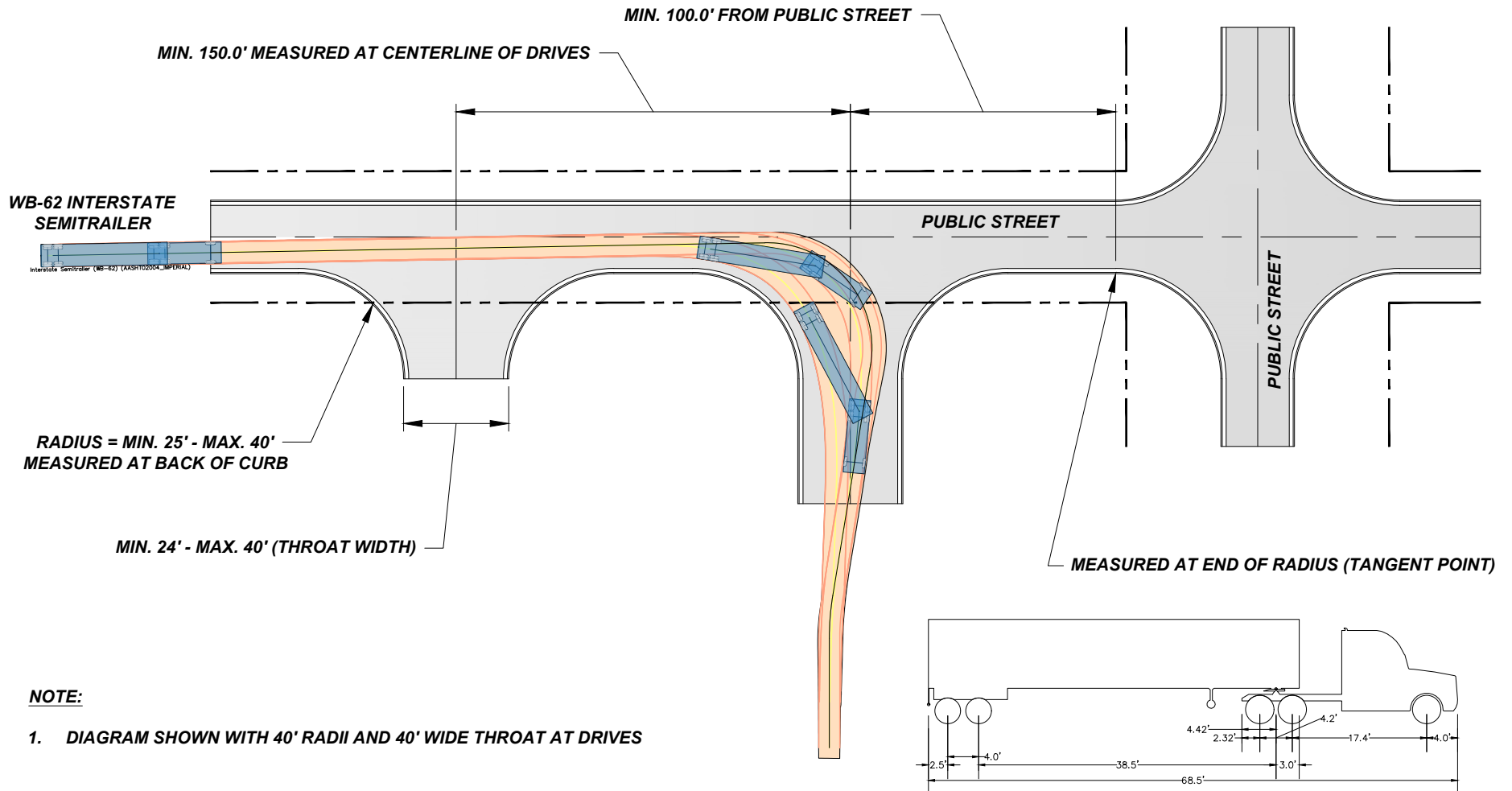
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CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.

Drawing Number:
RD-203



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COMMERCIAL/INDUSTRIAL DRIVE DIAGRAM

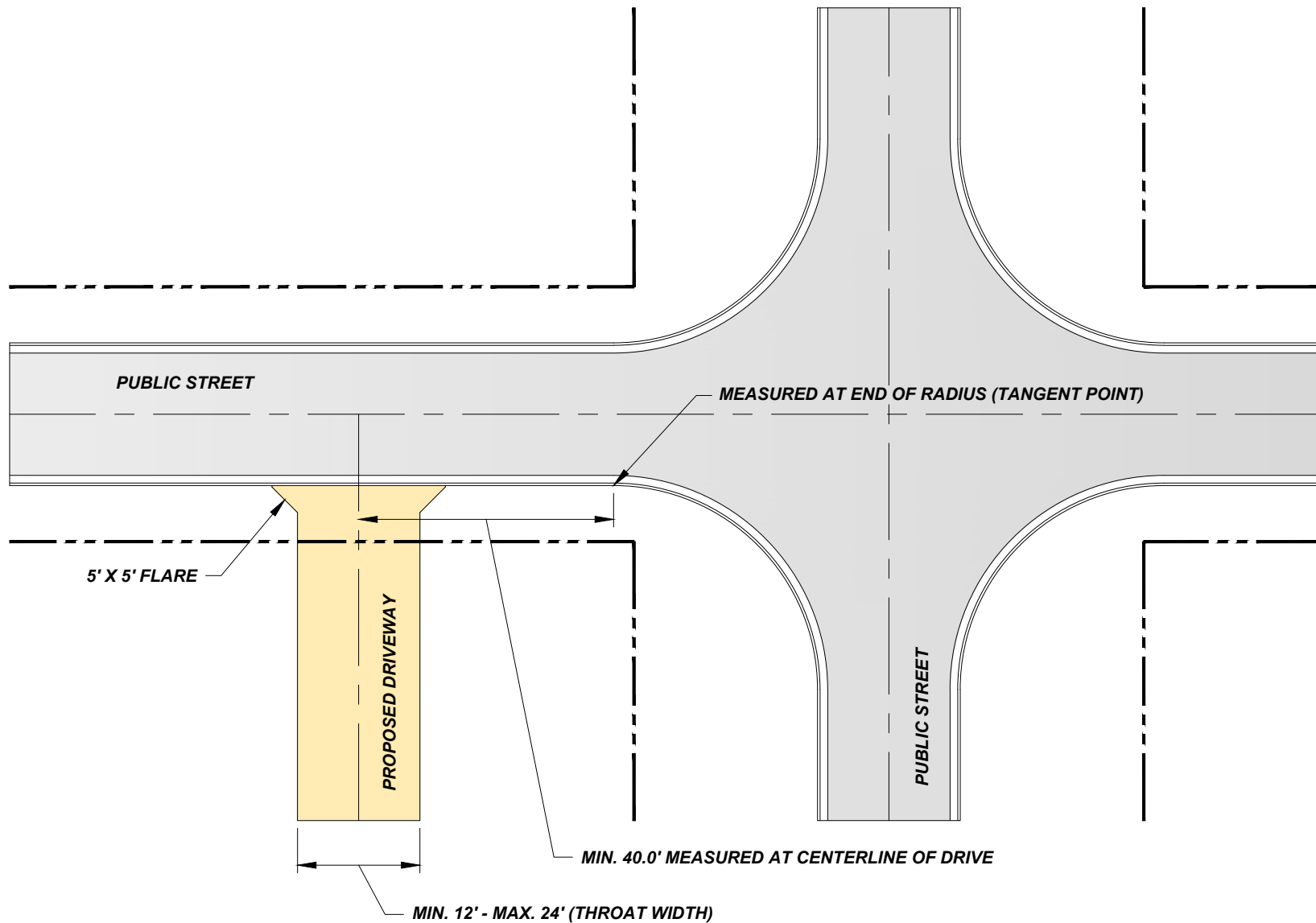
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CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
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 Drawing Number: **RD-301**



RESIDENTIAL DRIVE DIAGRAM

NOT TO SCALE

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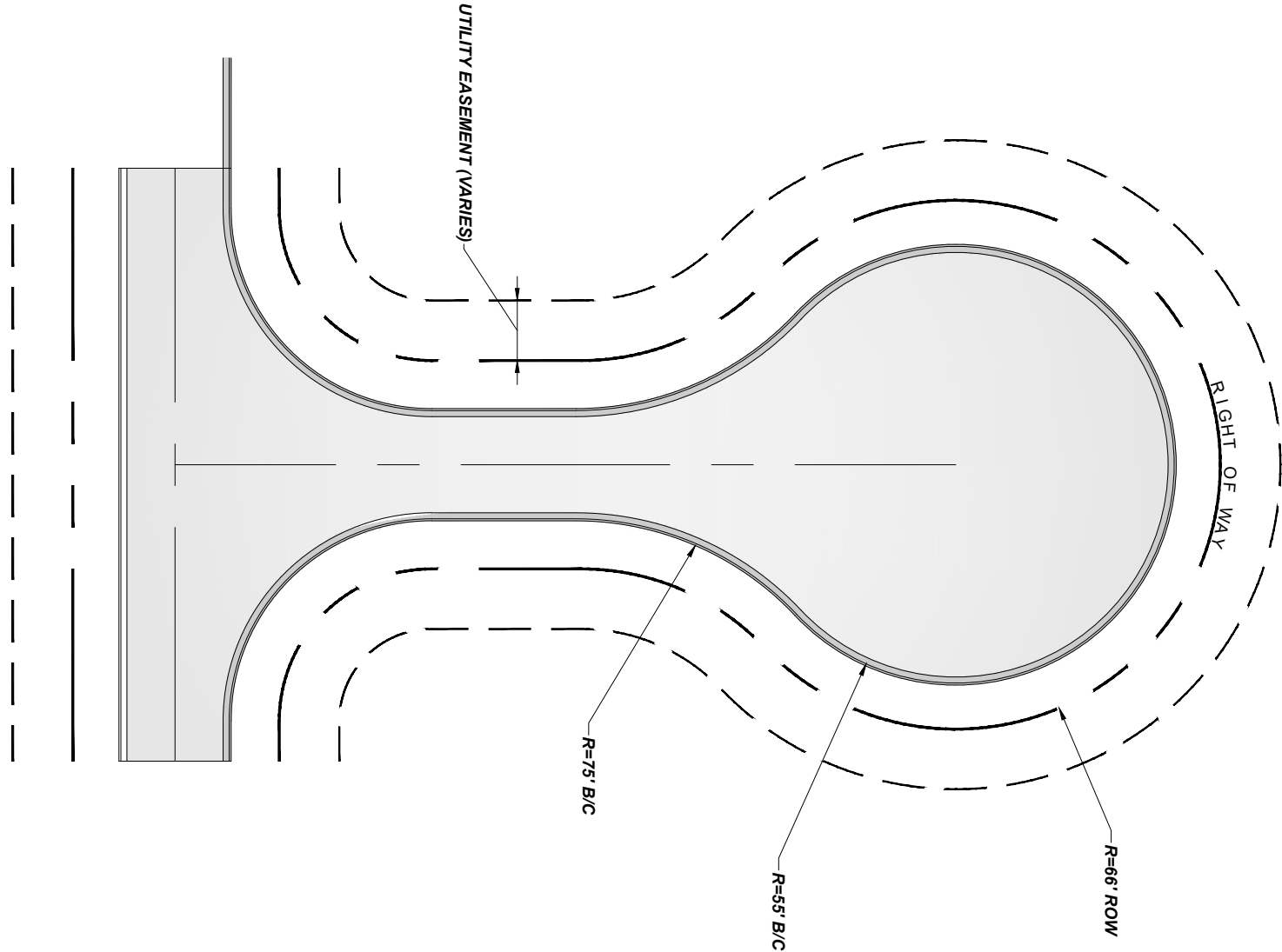
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CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman	Approved By: D. Sailor, P.E.
Drawing Number: RD-302	

COMMERCIAL/INDUSTRIAL CUL-DE-SAC LAYOUT

NOT TO SCALE



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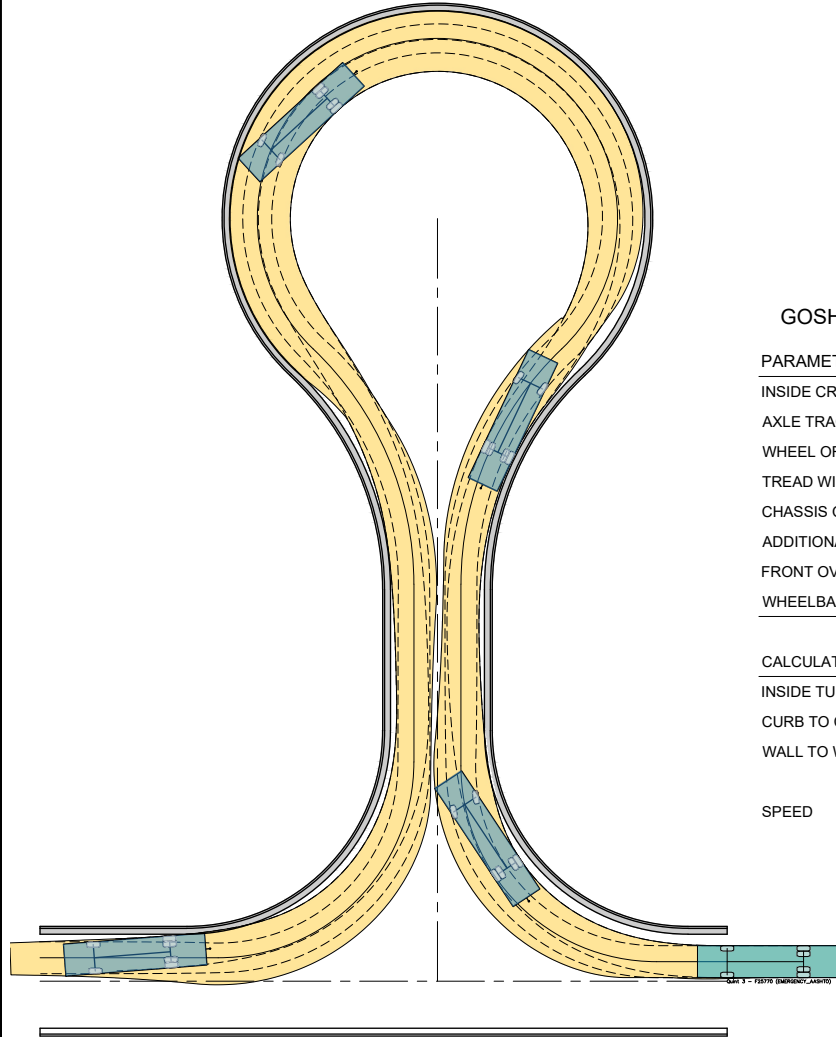
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 Web: gosheninindiana.org

Date Approved By The City Of Goshen Board Of Public Works & Safety and Approved Revisions

03/27/2025

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
 Approved By: D. Sailor, P.E.
 Drawing Number: **RD-303**



GOSHEN FIRE DEPARTMENT - QUINT 3

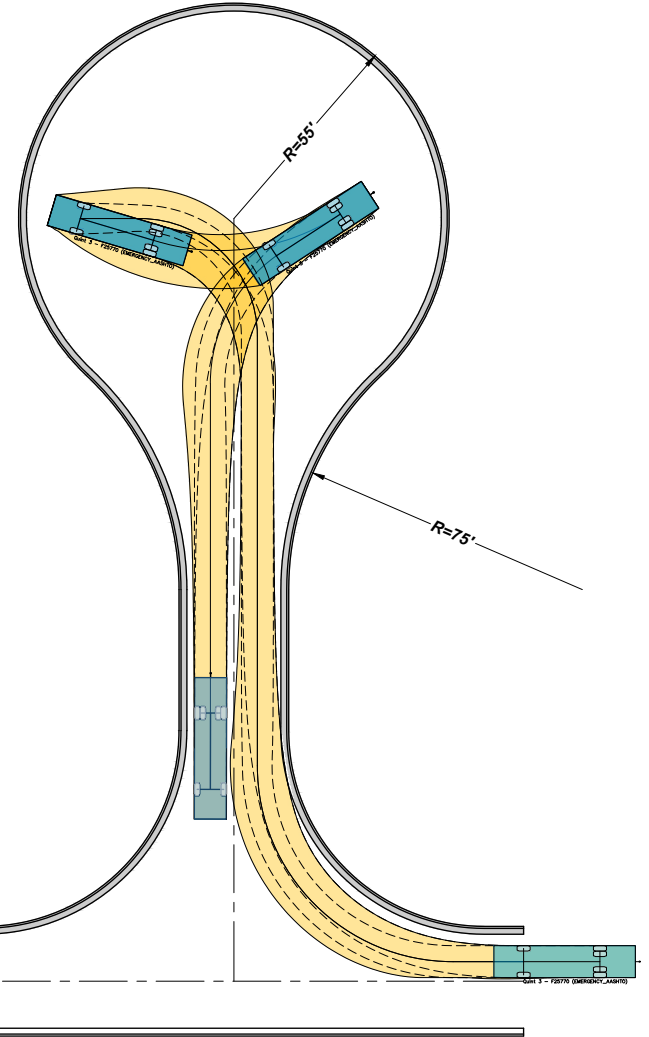
PARAMETERS

INSIDE CRAMP ANGLE:	45.00	DEGREE
AXLE TRACK:	82.92	IN.
WHEEL OFFSET:	4.68	IN.
TREAD WIDTH:	16.60	IN.
CHASSIS OVERHANG:	78.00	IN.
ADDITIONAL BUMPER DEPTH:	13.00	IN.
FRONT OVERHANG:	91.00	IN.
WHEELBASE:	234	IN.

CALCULATED TURNING RADII

INSIDE TURN:	18 FT. 5 IN.
CURB TO CURB	33 FT. 11 IN.
WALL TO WALL:	38 FT. 7 IN.

SPEED 10 MPH



COMMERCIAL/INDUSTRIAL CUL-DE-SAC WITH QUINT

NOT TO SCALE

The City Of Goshen
Department of Public Works & Safety
Office of Engineering

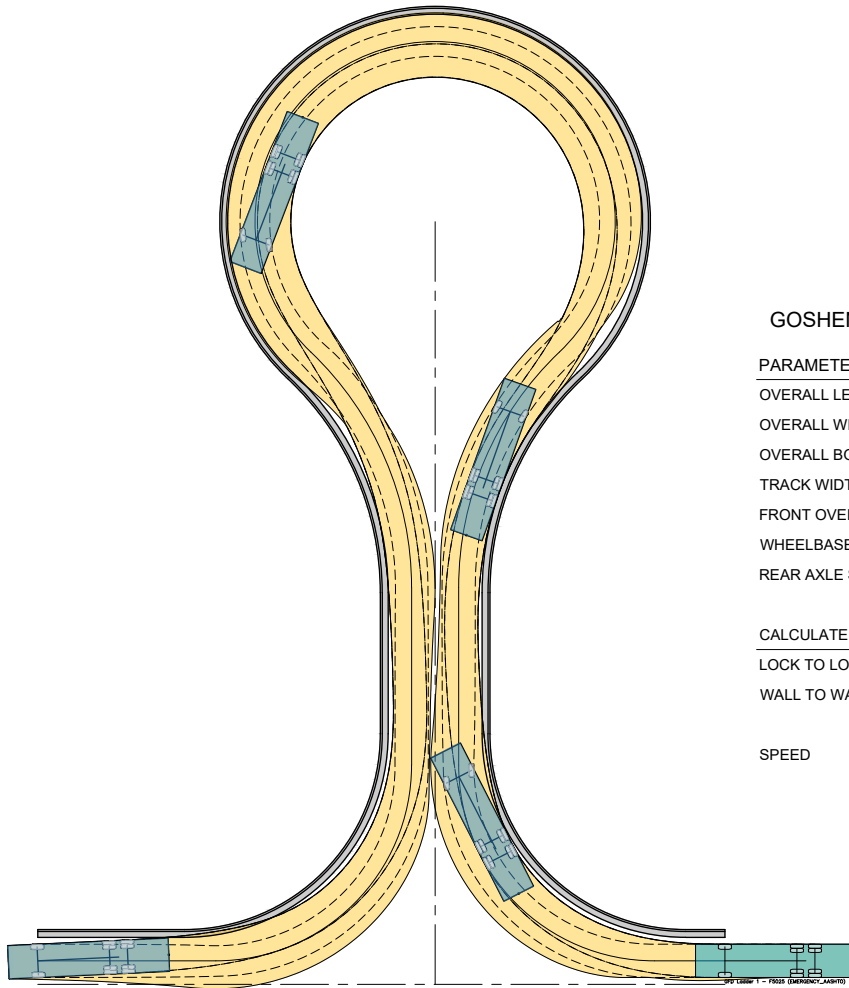
204 East Jefferson, Suite 1 Street, Goshen, Indiana 46528
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03/27/2025

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.
Drawing Number: RD-304



GOSHEN FIRE DEPARTMENT - LADDER 1

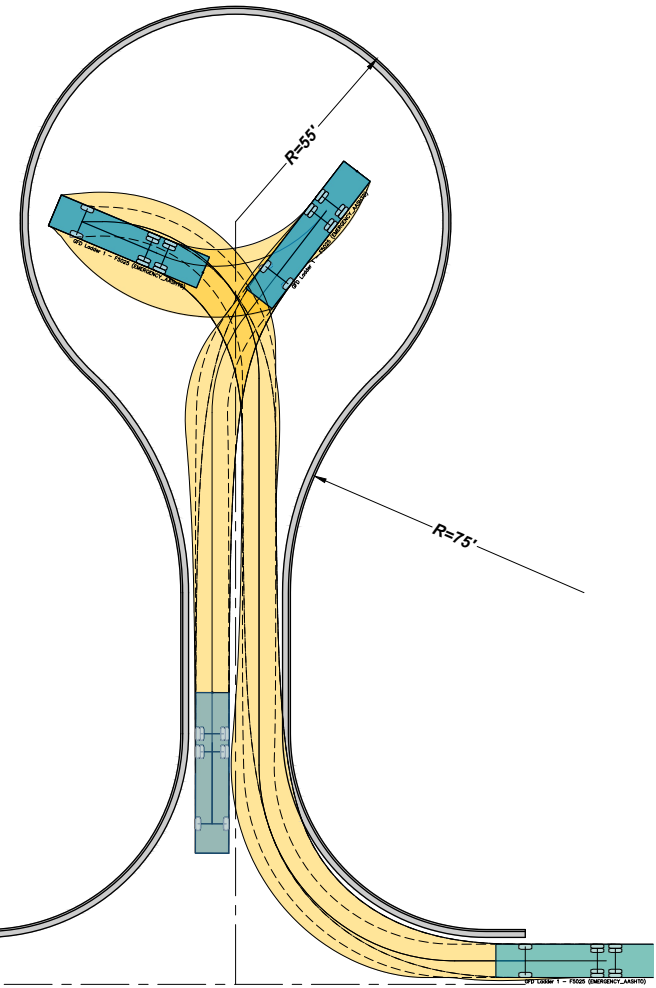
PARAMETERS

OVERALL LENGTH:	41.00	FT
OVERALL WIDTH:	8.50	FT.
OVERALL BODY HEIGHT:	10.25	FT.
TRACK WIDTH:	8.50	FT.
FRONT OVERHANG:	7.50	FT.
WHEELBASE:	18.40	FT.
REAR AXLE SPACING:	4.60	FT.

CALCULATED TURNING RADII

LOCK TO LOCK:	4.00	SEC.
WALL TO WALL:	48.50	FT.

SPEED 10 MPH



COMMERCIAL/INDUSTRIAL CUL-DE-SAC WITH LADDER 1

NOT TO SCALE

The City Of Goshen
Department of Public Works & Safety
Office of Engineering

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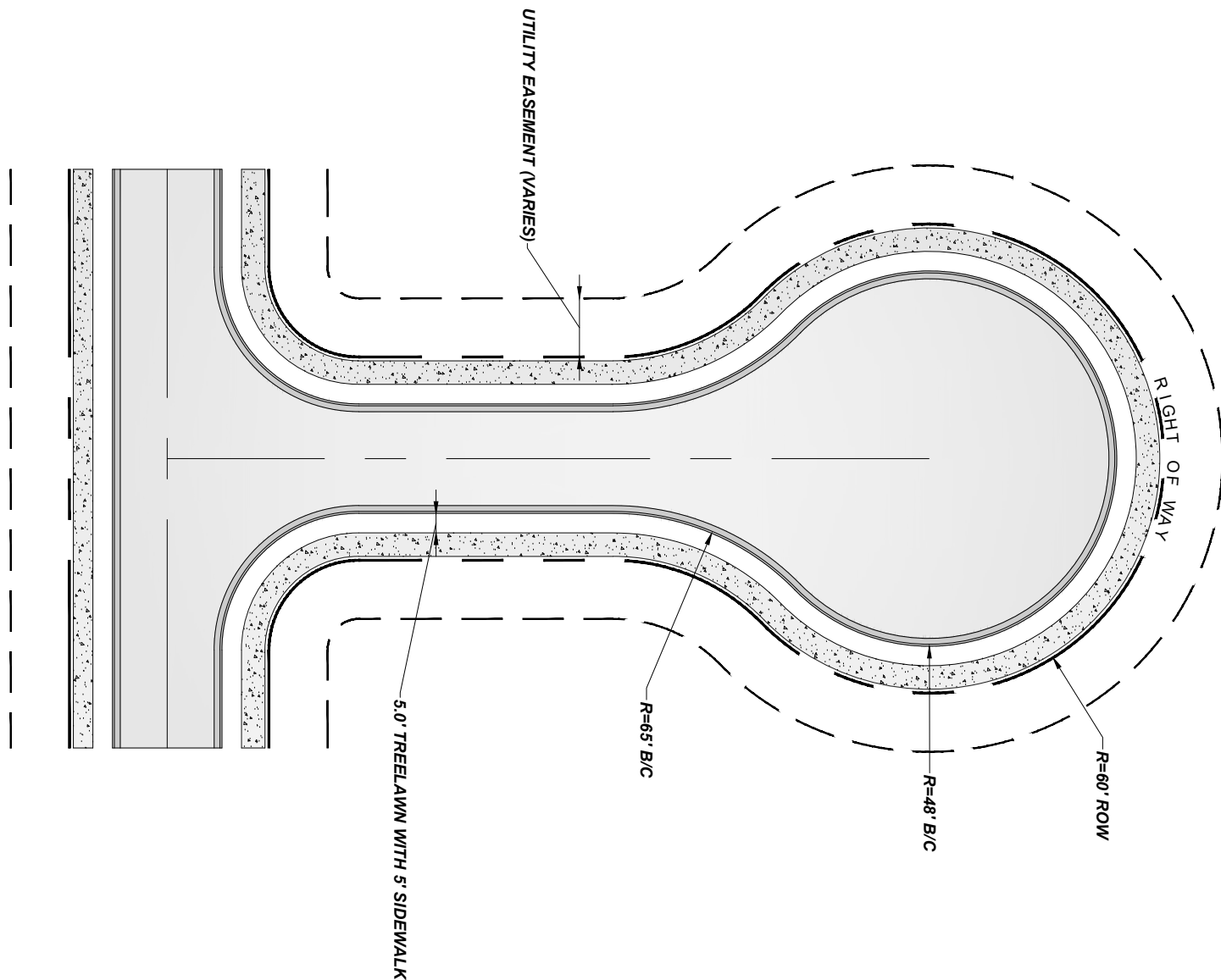
03/27/2025					

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.
Drawing Number: **RD-305**

RESIDENTIAL CUL-DE-SAC LAYOUT

NOT TO SCALE



The City Of Goshen
 Department of Public Works & Safety
 Office of Engineering

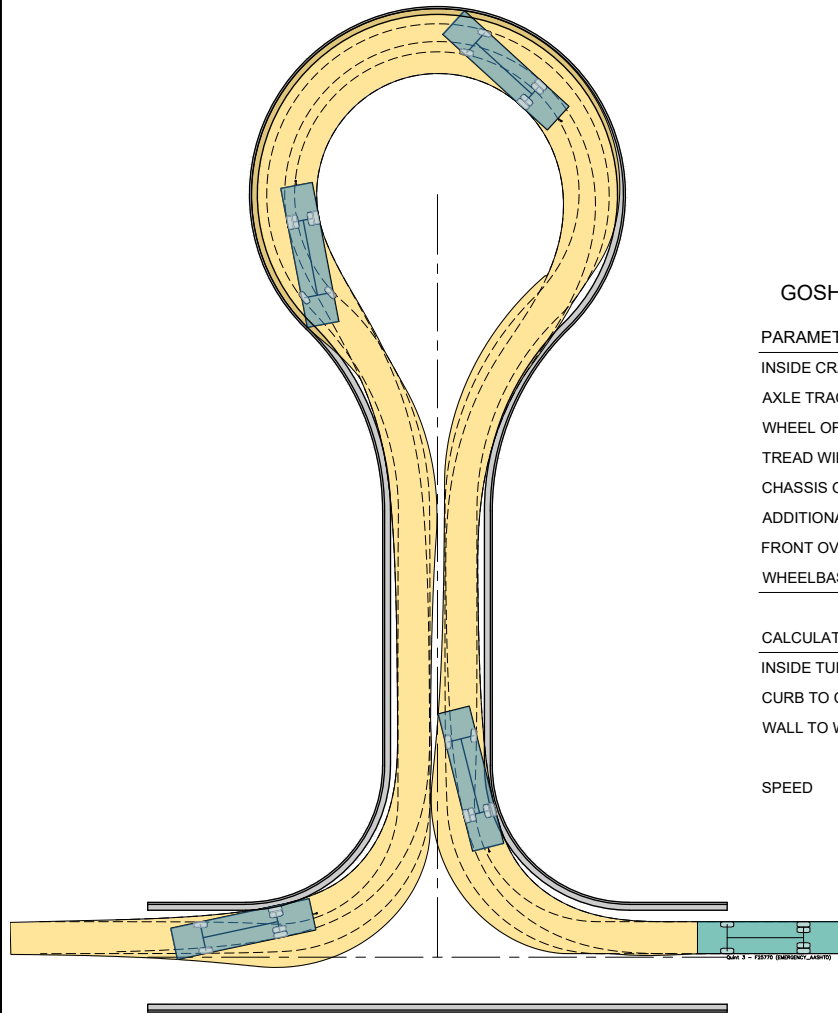
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03/27/2025

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
 Approved By: D. Sailor, P.E.
 Drawing Number: **RD-306**



GOSHEN FIRE DEPARTMENT - QUINT 3

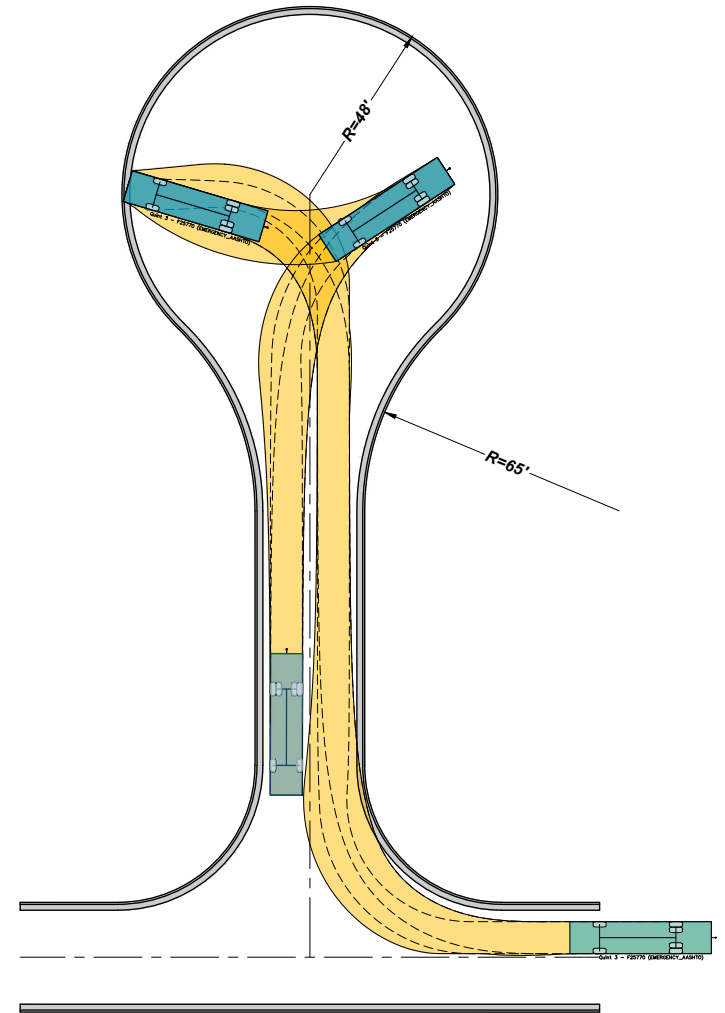
PARAMETERS

INSIDE CRAMP ANGLE:	45.00	DEGREE
AXLE TRACK:	82.92	IN.
WHEEL OFFSET:	4.68	IN.
TREAD WIDTH:	16.60	IN.
CHASSIS OVERHANG:	78.00	IN.
ADDITIONAL BUMPER DEPTH:	13.00	IN.
FRONT OVERHANG:	91.00	IN.
WHEELBASE:	234	IN.

CALCULATED TURNING RADII

INSIDE TURN:	18 FT. 5 IN.
CURB TO CURB	33 FT. 11 IN.
WALL TO WALL:	38 FT. 7 IN.

SPEED 10 MPH



RESIDENTIAL CUL-DE-SAC WITH QUINT

NOT TO SCALE

The City Of Goshen
Department of Public Works & Safety
Office of Engineering

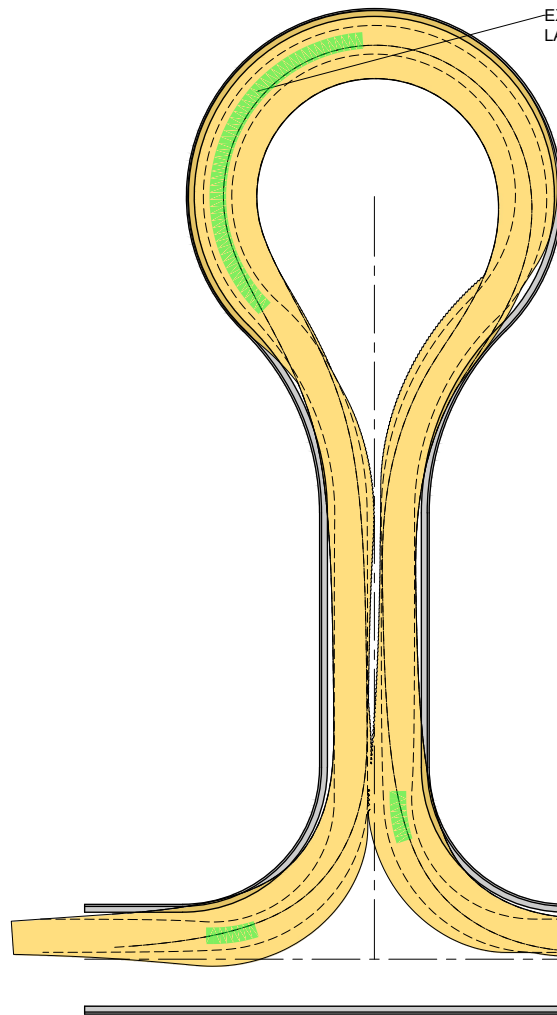
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03/27/2025					

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.
Drawing Number: **RD-307**



EXCEEDS MIN. TURN RADIUS (GREEN AREAS)
LANE ENCROACHMENT OR MULTI-POINT TURN ANTICIPATED

GOSHEN FIRE DEPARTMENT - LADDER 1

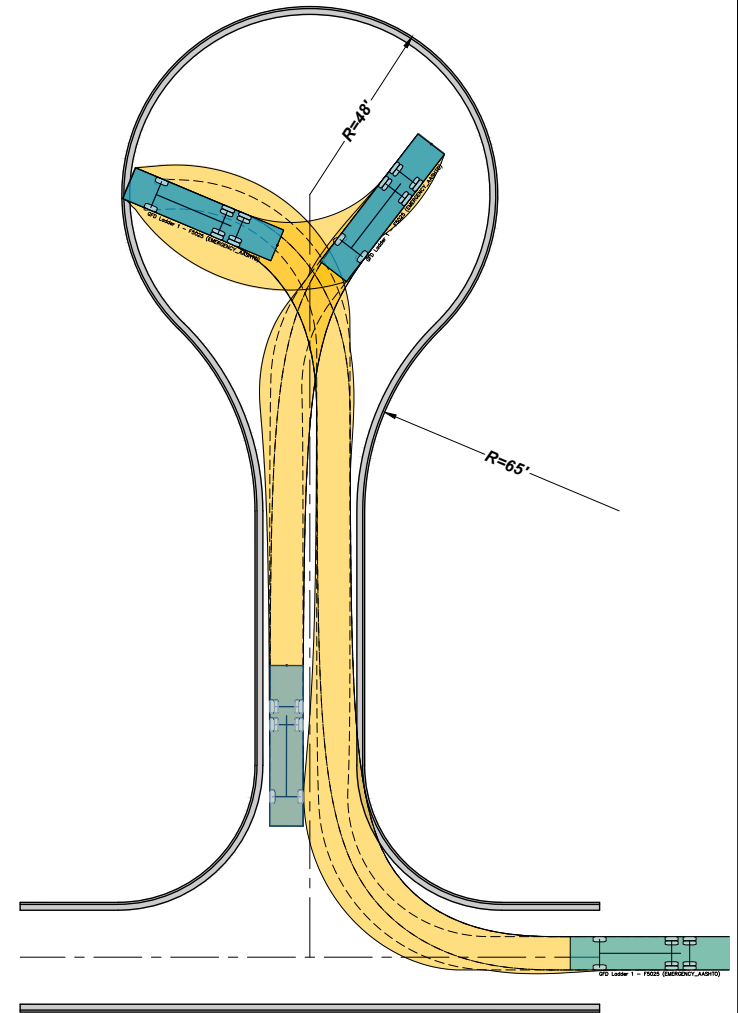
PARAMETERS

OVERALL LENGTH:	41.00	FT
OVERALL WIDTH:	8.50	FT.
OVERALL BODY HEIGHT:	10.25	FT.
TRACK WIDTH:	8.50	FT.
FRONT OVERHANG:	7.50	FT.
WHEELBASE:	18.40	FT.
REAR AXLE SPACING:	4.60	FT.

CALCULATED TURNING RADIUS

LOCK TO LOCK:	4.00	SEC.
WALL TO WALL:	48.50	FT.

SPEED 10 MPH



RESIDENTIAL CUL-DE-SAC WITH LADDER 1

NOT TO SCALE

The City Of Goshen
Department of Public Works & Safety
Office of Engineering

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03/27/2025					

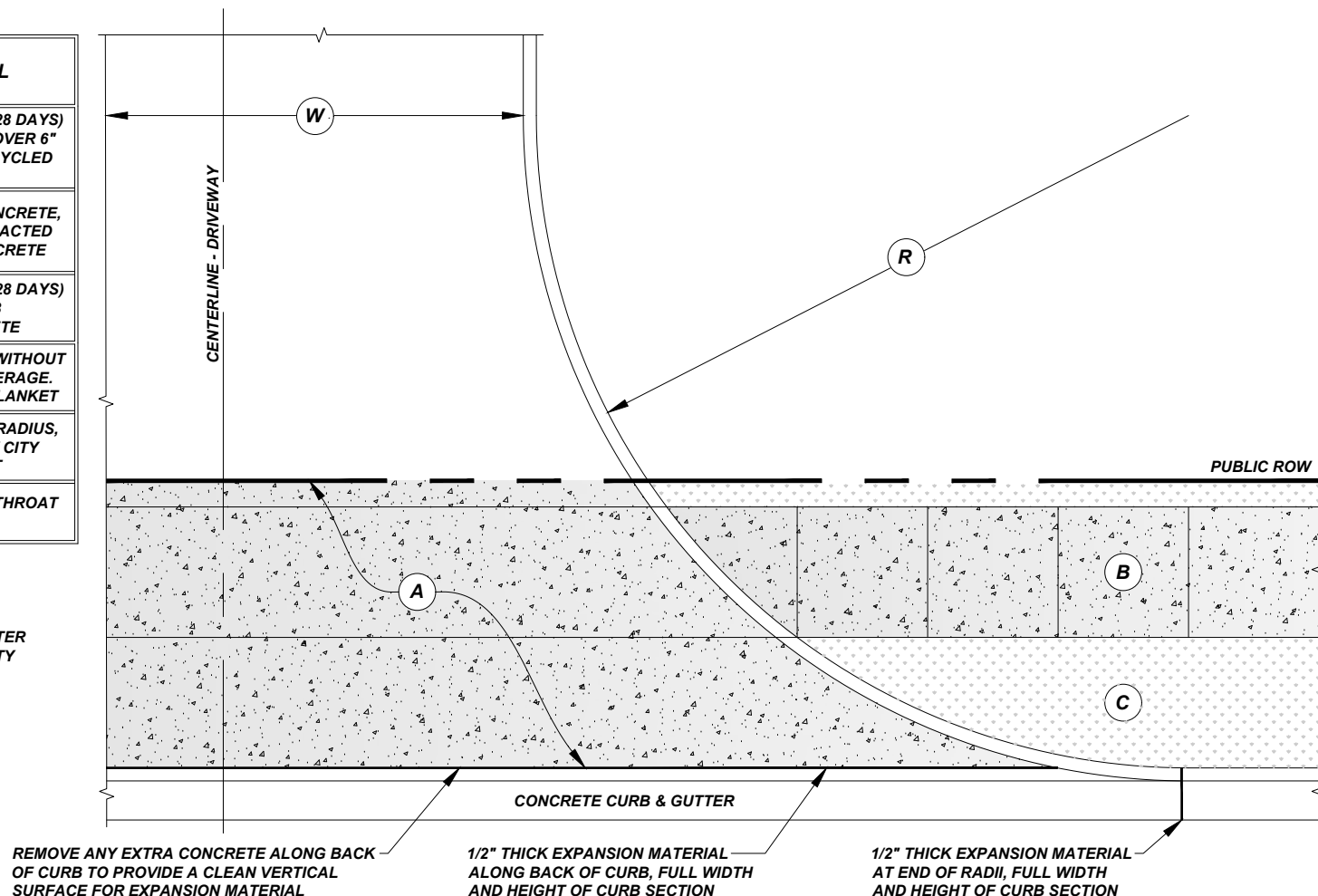
CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.
Drawing Number: RD-308

COMMERCIAL AND INDUSTRIAL	
A	6" CLASS 'A' CONCRETE, 4,000 PSI (28 DAYS) REINFORCED WITH 4X4 W4.0/W4.0 OVER 6" COMPACTED No. 53 CRUSHED/RECYCLED CONCRETE
A	9" CLASS 'A' NON-REINFORCED CONCRETE, 4,000 PSI (28 DAYS) OVER 6" COMPACTED No. 53 CRUSHED/RECYCLED CONCRETE
B	4" CLASS 'A' CONCRETE, 4,000 PSI (28 DAYS) OVER 4" COMPACTED No. 53 CRUSHED/RECYCLED CONCRETE
C	4" CLEAN TOPSOIL, INDOT TYPE 'U' WITHOUT CLOVER, ACHIEVE 70% EVEN COVERAGE. COVER WITH EROSION CONTROL BLANKET
R	24.0' MINIMUM AND 40.0' MAXIMUM RADIUS, APPROVAL OF LARGER RADII BY CITY ENGINEERING DEPARTMENT
W	24.0' MINIMUM AND 40.0' MAXIMUM THROAT WIDTH

NOTE:

1. FOR DRIVE APPROACH WIDTHS GREATER THAN THE 40' MAXIMUM, THE PROPERTY OWNER WILL BE REQUIRED TO APPLY FOR A VARIANCE



COMMERCIAL/INDUSTRIAL DRIVEWAY LAYOUT

NOT TO SCALE

The City Of Goshen
Department of Public Works & Safety
Office of Engineering

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Phone: 574-534-2201 Fax: 574-533-8626
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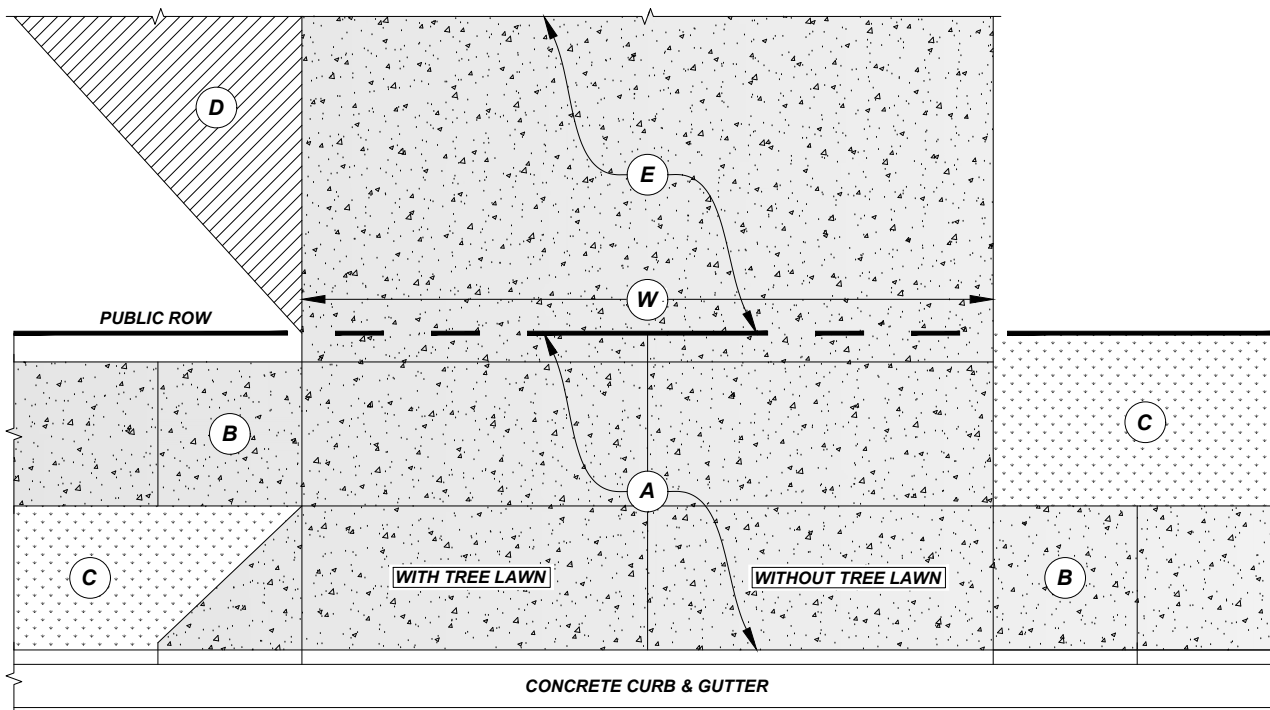
03/27/2025

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.

Drawing Number:

RD-401



RESIDENTIAL DRIVEWAY LAYOUT

NOT TO SCALE

SINGLE FAMILY AND SINGLE FAMILY CONVERTED TO MULTI-UNIT	
A	6" CLASS 'A' CONCRETE, 4,000 PSI (28 DAYS) OVER 4" No. 53 CRUSHED/RECYCLED CONCRETE, COMPACTED
B	4" CLASS 'A' CONCRETE, 4,000 PSI (28 DAYS) OVER 4" No. 53 CRUSHED/RECYCLED CONCRETE, COMPACTED
C	4" CLEAN TOPSOIL, INDOT TYPE 'U' WITHOUT CLOVER, ACHIEVE 70% COVERAGE. COVER WITH EROSION CONTROL BLANKET
D	CONTACT CITY PLANNING & ZONING DEPARTMENT AT (574) 534-8626 FOR APPROVAL TO INCREASE DRIVEWAY WIDTH ON PRIVATE PROPERTY
E	PRIVATE DRIVEWAY SECTION
W	12.0' MINIMUM AND 24.0' MAXIMUM WIDTH AT THE RIGHT OF WAY LINE
DUPLEX SINGLE LOT OR COMMON PROPERTY LINE WHERE GARAGE ENTRANCES WARRANT	
W	40.0' MAXIMUM WHERE

NOTE:

- FOR DRIVE APPROACH WIDTHS GREATER THAN THE 24' MAXIMUM, THE PROPERTY OWNER WILL BE REQUIRED TO APPLY FOR A VARIANCE

The City Of Goshen
Department of Public Works & Safety
Office of Engineering

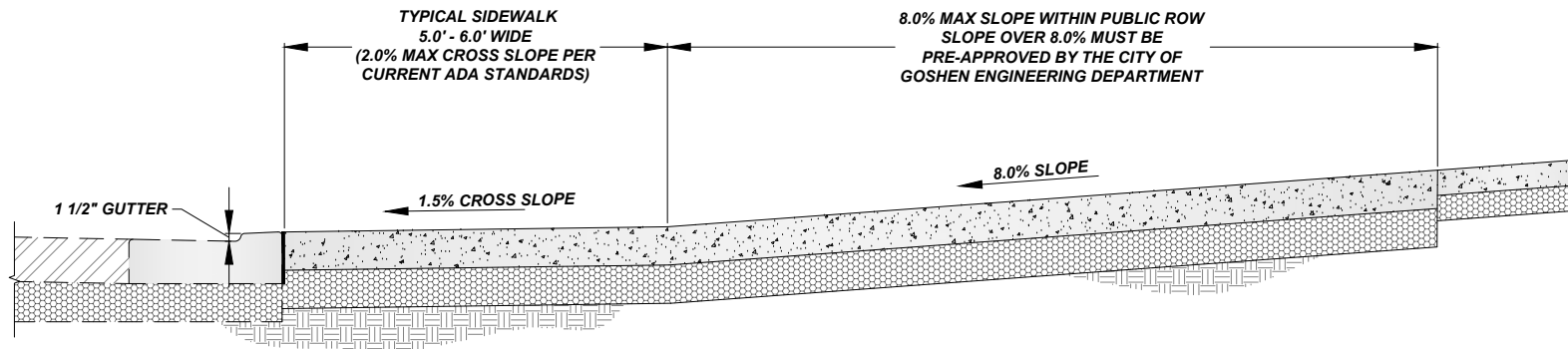
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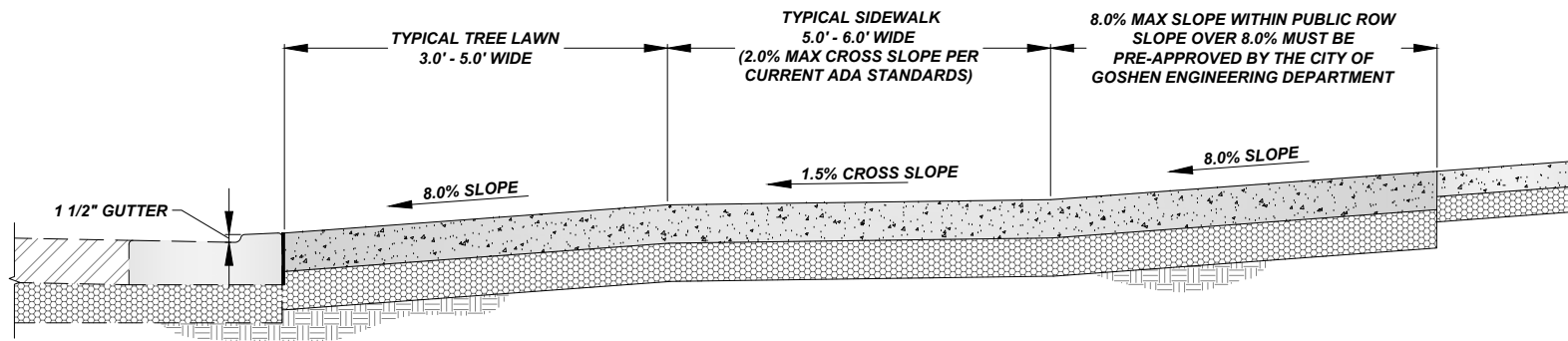
03/27/2025

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.
Drawing Number: **RD-402**



DRIVE APPROACH WITH SIDEWALK BEHIND CURB



DRIVE APPROACH WITH SIDEWALK AND TREE LAWN

**TYPICAL RESIDENTIAL/COMMERCIAL/INDUSTRIAL DRIVEWAY
APPROACH PROFILE**

NOT TO SCALE

The City Of Goshen
Department of Public Works & Safety
Office of Engineering

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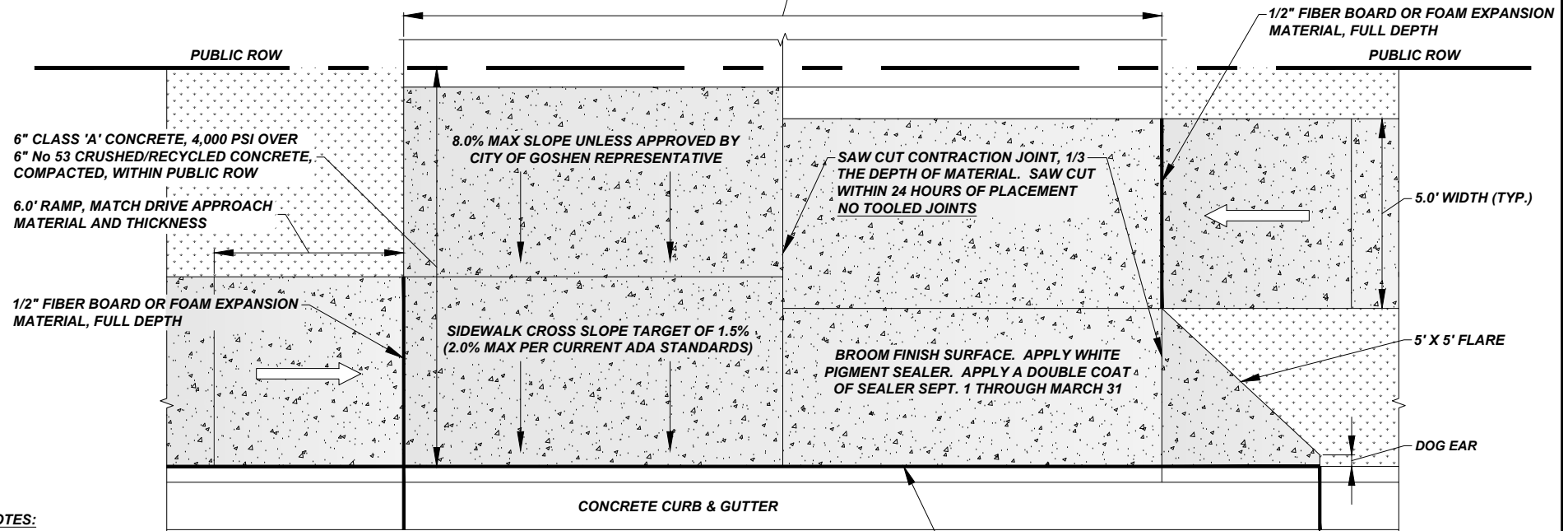
Date Approved By The City Of Goshen Board Of Public Works & Safety and Approved Revisions

03/27/2025

**CITY OF GOSHEN, INDIANA
STANDARD DETAIL**

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.
Drawing Number: **RD-403**

SINGLE FAMILY AND SINGLE FAMILY CONVERTED TO MULTI-UNIT: 24' MAX. WIDTH AT PUBLIC ROW
 MULTI-UNIT WITHIN SINGLE LOT: 40' MAX. WIDTH AT PUBLIC ROW
 DUPLEX WITH COMMON PROPERTY LINE: 40' MAX. WIDTH AT PUBLIC ROW



NOTES:

1. A PRE-POUR AND POST-POUR INSPECTION IS REQUIRED FOR ACCEPTANCE OF THE DRIVE APPROACH. CALL THE CITY ENGINEERING DEPARTMENT TO SCHEDULE AN INSPECTION AT (574) 534-2201 OR ONLINE, 48 HOUR NOTICE REQUIRED FOR SCHEDULING
2. DRIVE APPROACH - 6" CLASS 'A' CONCRETE, 4,000 PSI OVER 6" No 53 CRUSHED/RECYCLED CONCRETE, COMPACTED
3. SIDEWALK - 4" CLASS 'A' CONCRETE, 4,000 PSI OVER 4" No. 53 CRUSHED/RECYCLED CONCRETE, COMPACTED
4. APPLY WHITE PIGMENT SEALER. APPLY A DOUBLE COAT OF SEALER BEGINNING SEPT. 1 THROUGH MARCH 31
5. RESTORE YARD AREAS WITH 4" OF CLEAN TOPSOIL, INDOT TYPE 'U' WITHOUT CLOVER, COVER WITH EROSION CONTROL BLANKET. LEAVE TOP SOIL SLIGHTLY LOWER THAN SIDEWALK AND DRIVE APPROACH FOR DRAINAGE. THE CONTRACTOR IS RESPONSIBLE TO ACHIEVE 70% EVEN COVERAGE
6. NO REINFORCEMENT IS REQUIRED BUT MAY BE PLACED. CURB AND DRIVE APPROACH MUST REMAIN SEPARATE TO ALLOW FOR MOVEMENT
7. THE SUB-GRADE/SUB-BASE CAN NOT BE FROZEN OR WET WHEN THE CONCRETE IS PLACED. IF THE TEMPERATURES WILL CAUSE FROST IN THE GROUND, BLANKETS NEED TO BE PLACED OVER THE SUB-BASE TO PREVENT FREEZING
8. IF THE FINAL FINISH HAS CROSS SLOPES OVER 2.0% WITHIN THE SIDEWALKS, THOSE AREAS MUST BE REMOVED AND RE-POURED. INCLUDING THE SIDEWALK THROUGH THE DRIVE APPROACH. CROSS SLOPES ARE CHECKED WITH A 2' DIGITAL LEVEL
9. EXISTING CURB MAY NOT BE HORIZONTALLY SAW CUT TO DEPRESS THE CURB. THE CURB MUST BE REMOVED AND RE-POURED WITH NEW PROFILE

1/2" FIBER BOARD OR FOAM EXPANSION MATERIAL, FULL DEPTH. INSTALL BETWEEN CURB AND SIDEWALK. TOP OF EXPANSION MATERIAL SHALL BE EVEN WITH TOP OF CURB AND SIDEWALK

1/2" FIBER BOARD OR FOAM EXPANSION MATERIAL, FULL DEPTH AND WIDTH, SINGLE PIECE

The City Of Goshen
 Department of Public Works & Safety
 Office of Engineering

RESIDENTIAL DRIVEWAY APPROACH

NOT TO SCALE

204 East Jefferson, Suite 1 Street, Goshen, Indiana 46528
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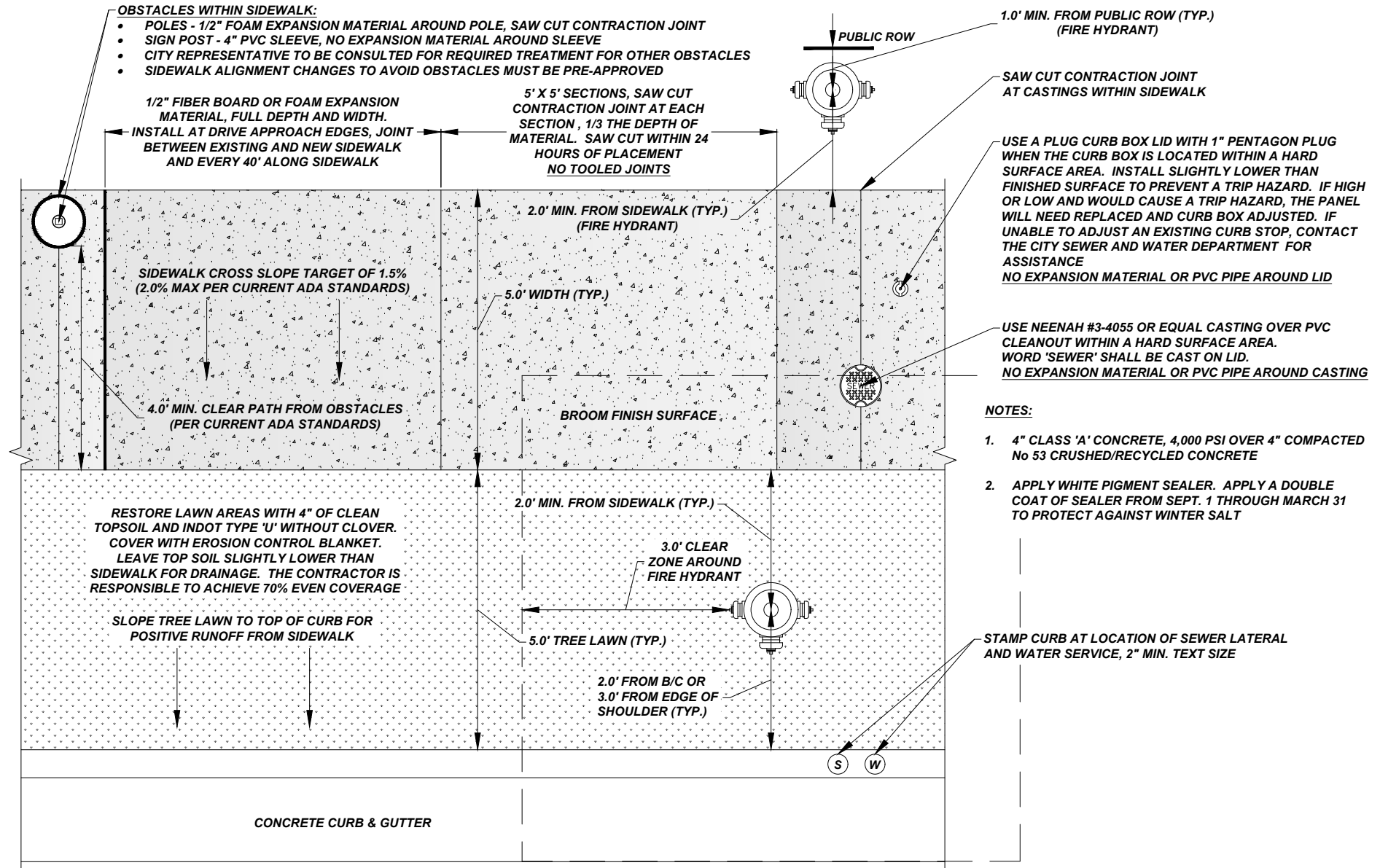
Date Approved By The City Of Goshen Board Of Public Works & Safety and Approved Revisions

03/27/2025

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
 Approved By: D. Sailor, P.E.

Drawing Number:
RD-404



SIDEWALK WITH TREE LAWN

NOT TO SCALE

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Office of Engineering

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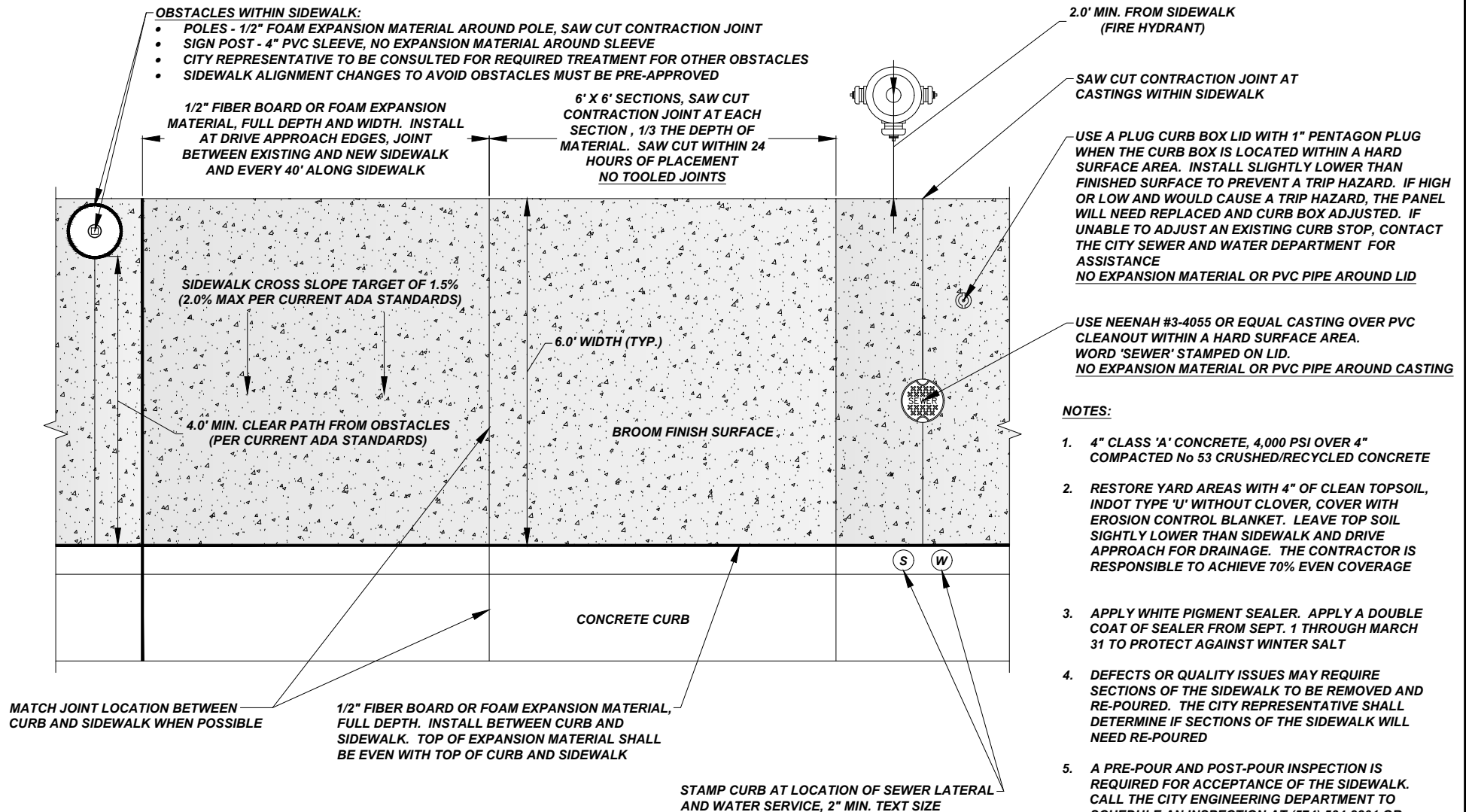
03/27/2025

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.

Drawing Number:

RD-405



SIDEWALK ADJACENT TO CONCRETE CURB

NOT TO SCALE

The City Of Goshen
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Office of Engineering

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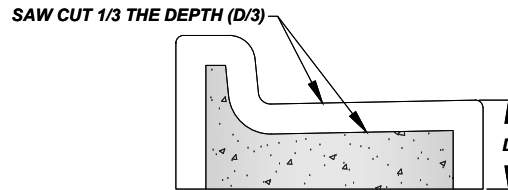
03/27/2025

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.

Drawing Number:

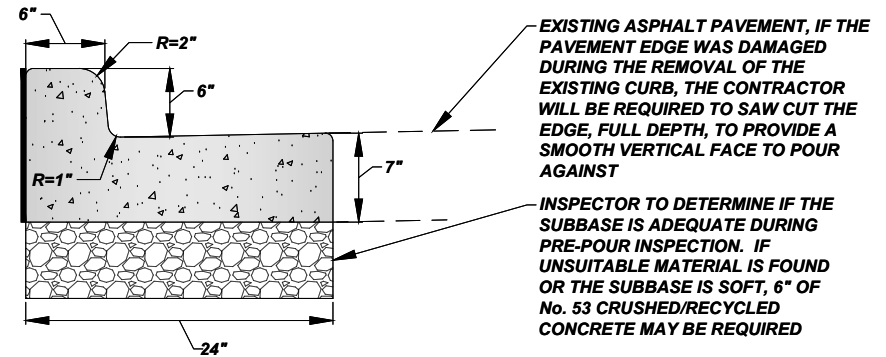
RD-406



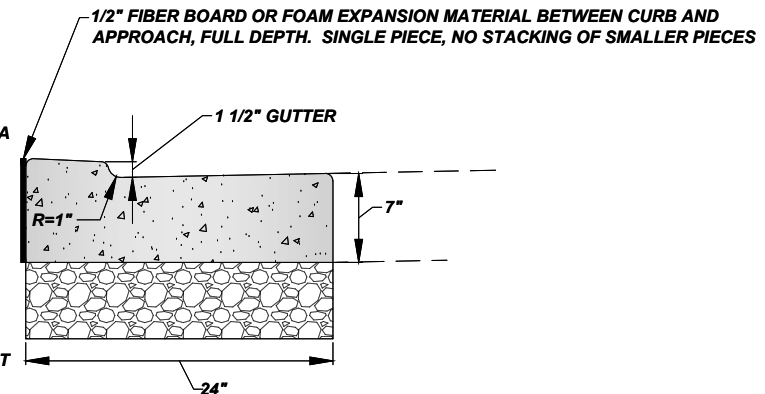
NOTES:

1. CLASS 'A' CONCRETE, 4,000 PSI
2. **EXPANSION JOINT LOCATIONS:**
STRAIGHT CURB SECTION - EVERY 40'
ROADWAY CURVE - BEGINNING POINT, MID POINT, END POINT AND EVERY 40' ALONG CURVE
INTERSECTION RADIUS - BEGINNING POINT, MID POINT, END POINT WITHOUT ADA RAMP OR SIDEWALK.
EACH SIDE OF RAMP
CURB CASTING - 4' FROM EACH SIDE OF THE CASTING
EXPANSION MATERIAL MUST BE FULL DEPTH AND WIDTH OF CURB SECTION OR WILL REQUIRE REPLACED
3. **CONTRACTION JOINT LOCATIONS:**
REQUIRED EVERY 10' AT 1/3 THE DEPTH, D/3, ALONG STRAIGHT CURB SECTIONS, ROADWAY CURVES AND INTERSECTION RADI
SAW CUT CONTRACTIONS JOINTS WITHIN 24 HOURS OF PLACEMENT
NO TOOLING OF CONTRACTIONS JOINTS IS ALLOWED
4. **REMOVE ALL EXCESS MATERIAL ALONG THE FACE OF THE CURB PRIOR TO MATERIAL SETTING TO ALLOW A CLEAN VERTICAL FACE TO PAVE OR POUR AGAINST. REMOVE EXCESS MATERIAL ALONG BACKSIDE OF CURB, AT DRIVE APPROACHES AND SIDEWALKS, TO ALLOW A CLEAN VERTICAL FACE FOR EXPANSION MATERIAL BETWEEN CONCRETE POURS**
6. **THE SUB-BASE/SUB-GRADE CAN NOT BE FROZEN OR WET WHEN THE CONCRETE IS PLACED. IF THE TEMPERATURES WILL CAUSE FROST IN THE GROUND, BLANKETS NEED TO BE PLACED OVER THE SUB-BASE/SUB-GRADE TO PREVENT FREEZING PRIOR TO PLACING THE CONCRETE**
4. **BROOM FINISH THE CONCRETE**
APPLY WHITE PIGMENT SEALER
APPLY A DOUBLE COAT OF SEALER FROM SEPT. 1 THROUGH MARCH 31 TO PROTECT AGAINST WINTER SALT
5. **RESTORE YARD AREAS WITH 4" OF CLEAN TOPSOIL, INDOT TYPE 'U' WITHOUT CLOVER, COVER WITH EROSION CONTROL BLANKET. THE CONTRACTOR IS RESPONSIBLE TO ACHIEVE 70% EVEN COVERAGE**

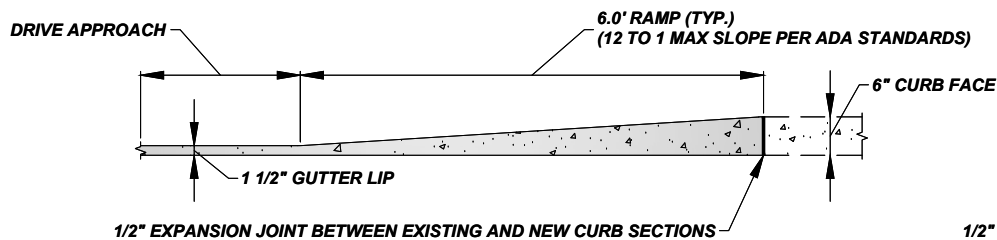
CONTRACTION JOINT X-SECTION



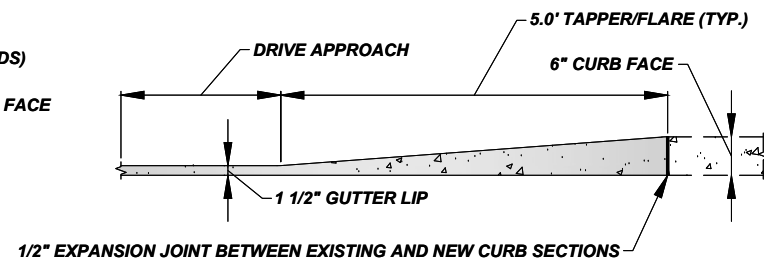
CONCRETE CURB & GUTTER



CONCRETE CURB & GUTTER AT DRIVE APPROACH



CONCRETE CURB & GUTTER TAPER PROFILE AT DRIVE APPROACH WITH SIDEWALK AND NO TREE LAWN



CONCRETE CURB & GUTTER TAPER PROFILE AT DRIVE APPROACH WITH SIDEWALK AND TREE LAWN

CONCRETE CURB & GUTTER AT DRIVEWAY APPROACH

NOT TO SCALE

The City Of Goshen
Department of Public Works & Safety
Office of Engineering

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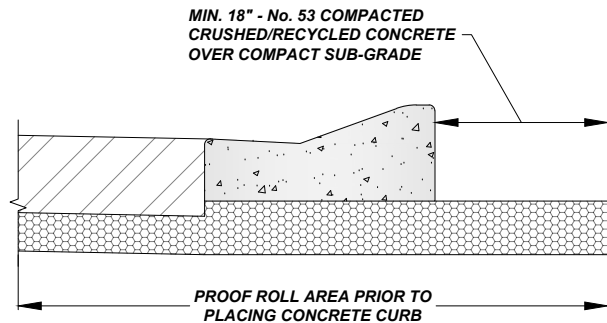
Date Approved By The City Of Goshen Board Of Public Works & Safety and Approved Revisions

03/27/2025

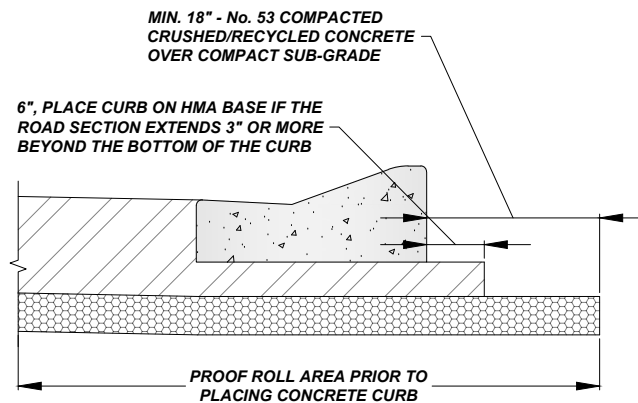
CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.

Drawing Number:
RD-407



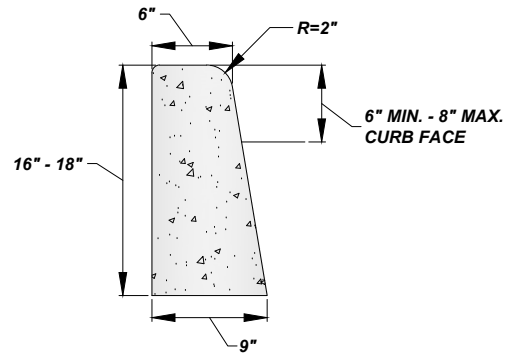
CONCRETE CURB INSTALLATION ON No. 53 BASE



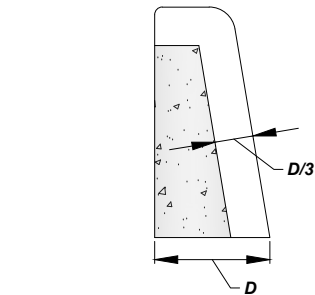
CONCRETE CURB INSTALLATION ON HMA BASE

STANDARD CONCRETE CURB

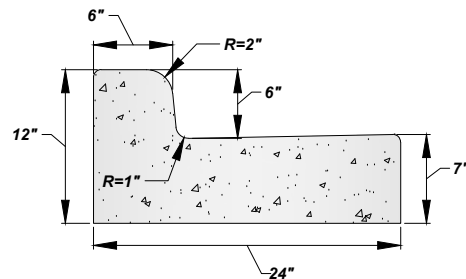
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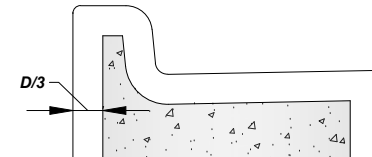
CONCRETE BARRIER CURB



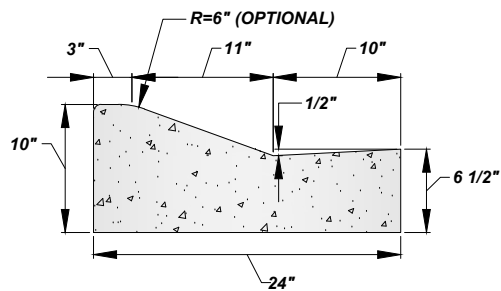
CONTRACTION JOINT SECTION



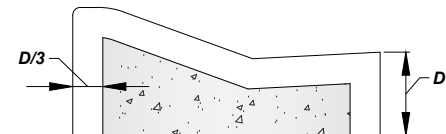
CONCRETE CURB & GUTTER



CONTRACTION JOINT SECTION



CONCRETE CURB & GUTTER



CONTRACTION JOINT SECTION

The City Of Goshen
Department of Public Works & Safety
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03/27/2025

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By: J. Hoffman
Approved By: D. Sailor, P.E.

Drawing Number:
RD-408

NOTES:

1. CLASS 'A' CONCRETE, 4,000 PSI
2. EXPANSION JOINT LOCATIONS:
STRAIGHT CURB SECTION - EVERY 40'
ROADWAY CURVE - BEGINNING POINT, MID POINT, END POINT AND EVERY 40' ALONG CURVE
INTERSECTION RADIUS - BEGINNING POINT, MID POINT, END POINT WITHOUT ADA RAMP OR SIDEWALK. EACH SIDE OF RAMP

CURB CASTING - 4' FROM EACH SIDE OF THE CASTING
EXPANSION MATERIAL MUST BE FULL DEPTH AND WIDTH OF CURB SECTION OR REPLACEMENT WILL BE REQUIRED
3. CONTRACTION JOINT LOCATIONS:
REQUIRED EVERY 10' AT 1/3 THE DEPTH, D/3, 1/8" MIN. TO 1/4" MAX. WIDTH, ALONG STRAIGHT CURB SECTIONS, ROADWAY
CURVES AND INTERSECTION RADII
SAW CUT CONTRACTION JOINTS WITHIN 24 HOURS OF PLACEMENT
NO TOOLING OF CONTRACTIONS JOINTS IS ALLOWED
4. REMOVE ALL EXCESS MATERIAL ALONG THE FACE OF THE CURB PRIOR TO MATERIAL SETTING TO ALLOW A CLEAN VERTICAL
FACE TO BUILD AGAINST. REMOVE EXCESS MATERIAL ALONG BACKSIDE OF CURB, AT DRIVE APPROACHES AND SIDEWALKS,
TO ALLOW A CLEAN VERTICAL FACE FOR EXPANSION MATERIAL AND MATERIAL TRANSITIONS
6. THE SUB-BASE/SUB-GRADE CANNOT BE FROZEN OR WET WHEN THE CONCRETE IS PLACED. IF THE TEMPERATURES WILL
CAUSE FROST IN THE GROUND, BLANKETS NEED TO BE PLACED OVER THE SUB-BASE/SUB-GRADE TO PREVENT FREEZING
PRIOR TO PLACING THE CONCRETE
4. BROOM FINISH REQUIRED
APPLY WHITE PIGMENT SEALER
APPLY A DOUBLE COAT OF SEALER FROM SEPT. 1 THROUGH MARCH 31 TO PROTECT AGAINST WINTER SALT
5. RESTORE YARD AREAS WITH 4" OF CLEAN TOPSOIL, BLUEGRASS, RYE AND FESCUE YARD SEED MIX, COVER WITH EROSION
CONTROL BLANKET. THE CONTRACTOR IS RESPONSIBLE TO ACHIEVE 70% EVEN COVERAGE

The City Of Goshen
Department of Public Works & Safety
Office of Engineering

204 East Jefferson, Suite 1 Street, Goshen, Indiana 46528
Phone: 574-534-2201 Fax: 574-533-8626
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Date Approved By The City Of Goshen Board Of Public Works & Safety and Approved Revisions

03/27/2025

CITY OF GOSHEN, INDIANA
STANDARD DETAIL

Drafted By:	Approved By:
J. Hoffman	D. Sailor, P.E.
Drawing Number:	
RD-408	

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